

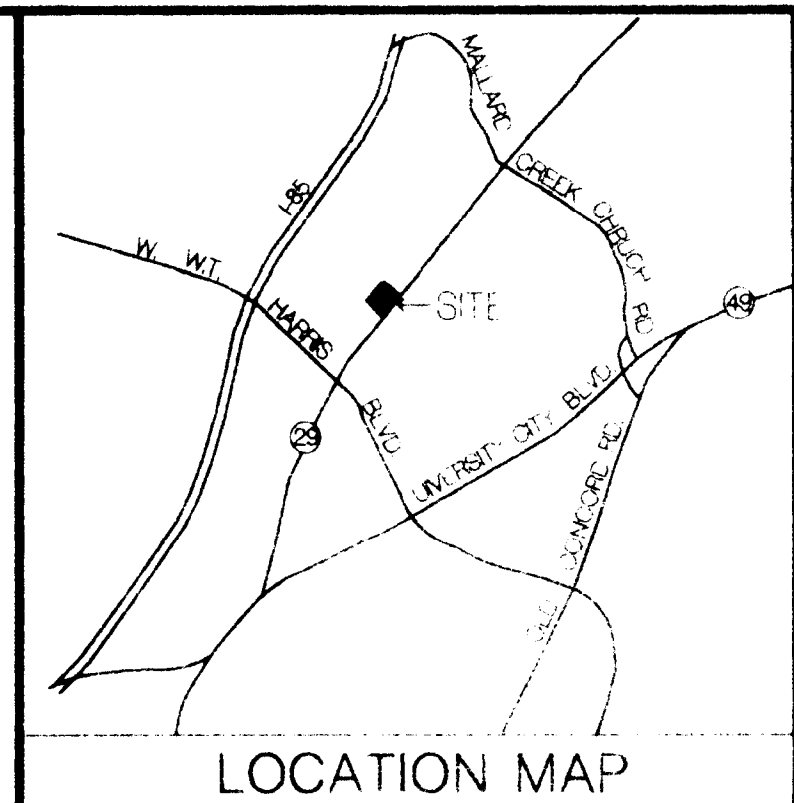
EXISTING ZONING: CC
 PROPOSED ZONING: CC SITE PLAN AMEND.

MAXIMUM BUILDING AREA: 173,300 SF,
 WHICH INCLUDES A ONE TIME 1,000 SQ. FT.
 ADDITION AS PER SECTION 6206(2)
 PERMITTED USES: ALL ALLOWED IN CC DISTRICT
 SITE ACERAGE: 16.731 AC.

- LEGEND**
- BUILDING ENVELOPE
 - PROPERTY BOUNDARY
 - OUT PARCEL AREA
 - SCREEN FENCE
 - BUFFER LINE

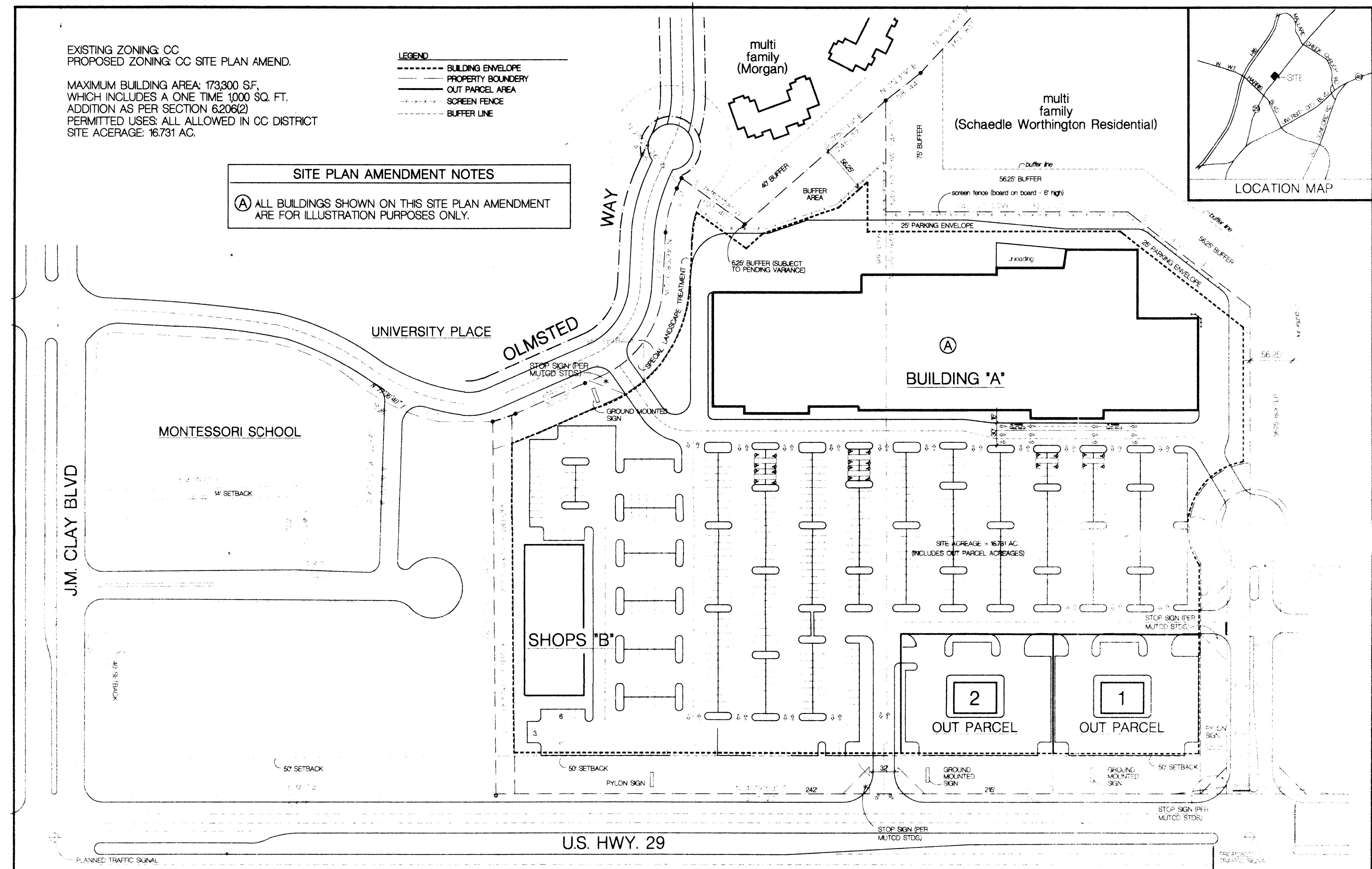
SITE PLAN AMENDMENT NOTES

(A) ALL BUILDINGS SHOWN ON THIS SITE PLAN AMENDMENT ARE FOR ILLUSTRATION PURPOSES ONLY.



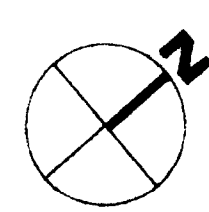
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MALLARD POINTE AT UNIVERSITY PLACE
 CHARLOTTE, N.C.

APPROVED BY CITY COUNCIL
 DATE October 16, 1995
 95-62



SITE PLAN AMENDMENT

FOR REZONING PETITION 95-62

FOR PUBLIC HEARING

scale: 1" = 60'

REVISIONS
SITE PLAN AMENDMENT
DRAWN BY
CHECKED BY
DATE
SCALE
51

DEVELOPMENT STANDARDS

Date: 10-3-95

MALLARD POINTE AT UNIVERSITY PLACE

DEVELOPMENT STANDARDS

Site Plan Amendment For Rezoning Petition 95-62

GENERAL PROVISIONS

These Development Standards apply to the property depicted on the Site Plan Amendment (the "Site") and represent a development concept with regard to the arrangement of buildings, parking, and circulation. Except as otherwise provided in this Site Plan Amendment (the "Site Plan Amendment"), all development standards established under the City of Charlotte Zoning Ordinance (the "Ordinance") for the Commercial Center District Classification shall be followed in connection with development taking place on the Site. The building configurations, placements, and sizes shown on the Site Plan Amendment are schematic in nature and may be altered or modified during design, development, and construction phases so long as the maximum building envelopes are not violated by final building placement. Parking/aisle layouts and internal circulation may also be modified to accommodate final building locations, tenant needs, final designs, site constraints, etc. Parking and vehicular circulation may occur within building envelope areas.

PERMITTED USES

- The total gross floor area of all structures placed on the Site (including detached buildings) cannot exceed 173,300 square feet.
- The building noted as SHOPS "B" on the Site Plan Amendment shall be a minimum of 10,000 SF in size, except that such restriction shall be eliminated if this building is one of the two outparcels listed under number 4 below.
- Any use (including any accessory use) which is permitted by right or under prescribed conditions in a Commercial Center District (CC) under the Ordinance shall be permitted.
- No more than two (2) outparcels may be developed along the US-29 road frontage on the Site.

SETBACKS

- Building and parking setbacks shall be a minimum of 50 feet off of the existing right-of-way for US-29 and per the zoning regulations along other property lines.
- Building and parking setbacks from all other streets shall be 35 feet.

BUFFERS/GREENWAYS

- The landscaped setback depicted along US-29 is subject to the following regulations:
 - A landscaped setback extending 50 ft. from the existing US-29 right-of-way will be retained except to the extent necessary to accommodate (i) pedestrian pathways, (ii) up to two (2) access point(s) along US-29 providing ingress/egress to the Site, (iii) two (2) outparcel ground mounted signs, (iv) one (1) pylon center identification sign and temporary signs (as permitted by the City of Charlotte Sign Ordinance), (v) sidewalks, gateway entrance features and utilities necessary to serve the Site, (vi) the clearing of undergrowth (trees smaller than 1 inch in caliper), (vii) clearing and grading required to meet transportation site line and accel/decel lane requirements, and (viii) the placement of trees and other plant materials required by applicable ordinances and as provided for as part of these standards where the existing foliage does not create an adequate visual screen.
- The buffer area between the Site and the adjoining multi-family zoned property (Schaeble Worthington Residential) is located on the multi-family property and shall be constructed pursuant to the conditional zoning of that property:
 - The shopping center to be constructed on the Site may open for business only after all Buffer improvements pertaining to landscaping and fencing have been completed in accordance with Note 10 of Petition No. 94-95, as amended by letter from Bob Young dated September 27, 1994.
 - Those plans required to obtain building permits for the shopping center will show the location of the Buffer, the landscaping, and the fencing.
 - The shopping center proper area will remain in its natural state until such time as construction activity commences.
- The remaining buffer area between the Site and the abutting Multi-Family parcel (Morgan), shall vary in width and shall be reduced to 6.25 feet as shown, provided a screening fence is installed as shown in accordance with a variance from the City of Charlotte Zoning Board of Adjustment which was granted for a portion of the buffer area on June 27, 1995.

- Special streetscape treatment of portions of the 35 foot setback along Olmstead Way, adjacent to the side of the shopping center as noted, shall include landscaping this area to meet class "C" planting standards.

LANDSCAPING

- All landscaping will meet the requirements of the City of Charlotte's Tree Ordinance.
- The landscape design within the landscaped setback established along US-29 will continue the pattern of landscaping, including type and spacing, which has been planted along US-29 in front of University Place and generally conform with the University Place Design Guidelines.
- Landscaping within the parking areas of the Site shall be a minimum of 8% of such parking area.

PARKING

- Each of the parking areas depicted on the Site Plan Amendment may vary in size and location, but in all events, off-street parking will meet the minimum standards established under the Ordinance.
- Parking areas may be constructed inside and outside building envelopes.

LIGHTING

- A lighting system will be employed uniformly throughout the Site.
- All direct lighting within the Site shall be designed in such a manner as to eliminate direct illumination upon adjacent properties.
- The maximum height of parking lot lights shall be 35 feet, including the mounting base.

SIGNS

- All signs placed on the Site will be erected in accordance with the requirements of the City of Charlotte Sign Ordinance.
- All outparcel building and tenant identification signs or graphics will be complementary in scale and appearance to the structures which they identify, and each sign affixed to a structure will be compatible with its architectural design.
- Two (2) permanent pylon type project identification signs not to exceed 30 ft. in height and two (2) outparcel ground mounted signs not to exceed 4 ft. in height as permitted by the Sign Ordinance and directional signs may be located on the Site. The project identification sign located on Olmstead Way shall be a ground mounted sign not to exceed 4 ft. in height as permitted by the Sign Ordinance.
- Project identification signs and gateway entrance features may be located at the access points to the Site along US-29 and Olmstead Drive.
- Temporary project signs may be located on the Site along US-29 within the setback established on the Site Plan Amendment, but must be removed no later than 60 days after issuance of the first certificate of occupancy for space within the parcel on which the project sign was erected.

ACCESS POINTS (DRIVEWAYS) AND ROAD IMPROVEMENTS

- The number of vehicular access points to the Site from US-29 shall be limited to two: namely, a right in/right out and a full access (median cut) for a new public street which will provide access to the northerly section of Site, as generally shown on the Site Plan Amendment. Access to the proposed Public Street will be allowed as shown on the Site Plan Amendment. The right in/right out entrance may be relocated along the US-29 road frontage subject to approval by the North Carolina Department of Transportation and the Charlotte Department of Transportation.
- The existing Olmstead Drive and Olmstead Way in University Place will be connected with the Site at ~~ONE (1) point~~ as generally shown on the Site Plan Amendment to provide vehicular access between University Place and the Site.
- The configurations of access points and driveways within the Site are subject to any minor modifications required to accommodate final site and architectural construction plans and designs and are further subject to approval by the North Carolina Department of Transportation, Charlotte Department of Transportation, and the City of Charlotte Engineering and Property Management Department.
- A third southbound lane of US-29 will be constructed along the Site as follows: Owner shall install curb and gutter and sidewalk necessary for a third southbound lane along US-29 in

front of the Site; the City of Charlotte Transportation Department shall, as per City policy and practice, install the necessary paving of such third lane from the existing edge of pavement to the curb and gutter installed by Owner.

DEDICATION OF ADDITIONAL RIGHT-OF-WAY

- Additional right-of-way required for accel/decel lanes to accommodate ingress/egress into and through the Site shall be dedicated at the time construction permits are issued.

FIRE PROTECTION

- Adequate fire protection in the form of fire hydrants will be provided in accordance with the Fire Marshal's specifications. Plans for each building will be submitted to the Fire Marshal's office for approval before construction of the building involved commences.

ARCHITECTURAL CONTROLS RELATING TO SITE

- The owners commit to design and construct the front, side, and rear elevations of the proposed shopping center buildings located on the Site such that their appearance will be compatible in quality with University Place.
- The principal materials used for the front and side building elevations will consist of brick, split face block, stucco or stucco appearing materials, or other materials customarily used in quality shopping centers. Any combination of these materials may be used.
- The material selected for the rear of the buildings will be painted to match the color of the predominant material used in the construction of the front and side elevations, subject to note #1 above.
- The designs of the buildings constructed within the Outparcels located on the Site will be generally compatible with the shopping center architectural design.
- No building constructed on the Site may contain more than two (2) stories or exceed 40 feet in height.
- All dumpster areas will be screened in accordance with the requirements of the Ordinance.
- Petitioner will complete final architectural plans and landscape plans for the entire site which will be presented to the Design Review Board for their review and approval.

STORMWATER MANAGEMENT

- Stormwater runoff from the Site will be managed through a variety of proven techniques which satisfy the standards imposed by the City of Charlotte. This may include off site detention.
- The buffer area will not be used for storm water management.

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DEVELOPMENT STANDARDS

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