

DEVELOPMENT DATA

TOTAL SITE AREA: 60.54 ACRES (EXISTING R/W EXCLUDED)
 EXISTING ZONING: R-3
 PROPOSED ZONING: INSTITUTIONAL (C/D)
 MAXIMUM NUMBER OF UNITS: 416
 PROPOSED UNIT TYPES:
 66 DETACHED OR ATTACHED INDEPENDENT LIVING UNITS (COTTAGE UNITS)
 270 INDEPENDENT LIVING APARTMENT UNITS
 80 ASSISTED LIVING OR SKILLED NURSING UNITS
 416 TOTAL UNITS MAXIMUM

DEVELOPMENT STANDARDS

A. PERMITTED USES

A retirement community for the elderly having no more than 416 living units, consisting of a maximum of 66 detached or attached dwelling units, 270 independent living units, and 80 assisted living or skilled nursing units, with associated support services and accessory uses, which may include dining facilities, exercise facilities, administrative offices, a library, staff lounge, emergency nursing and doctors offices, and other uses which are compatible with a private retirement community and permitted under the Ordinance in the Institutional District.

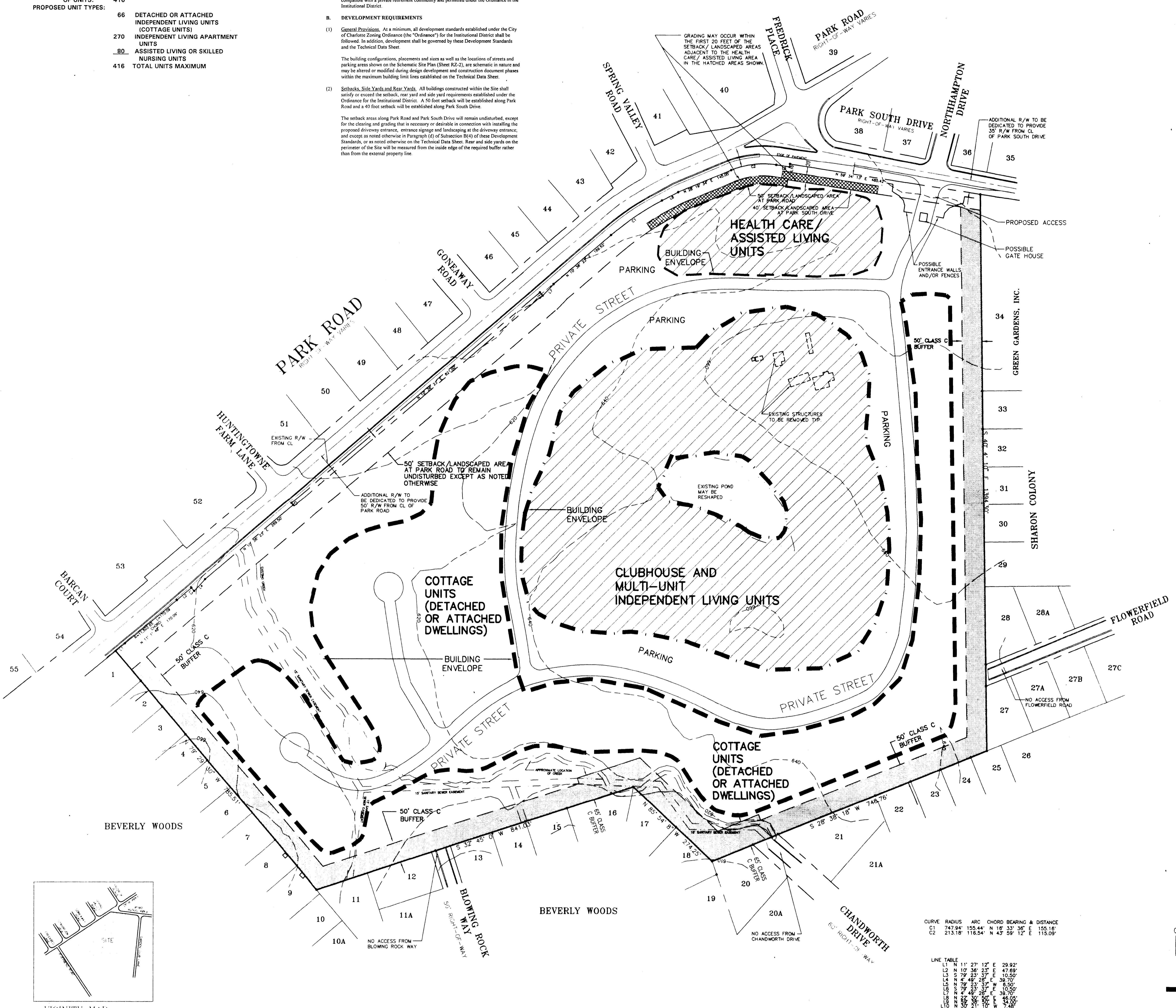
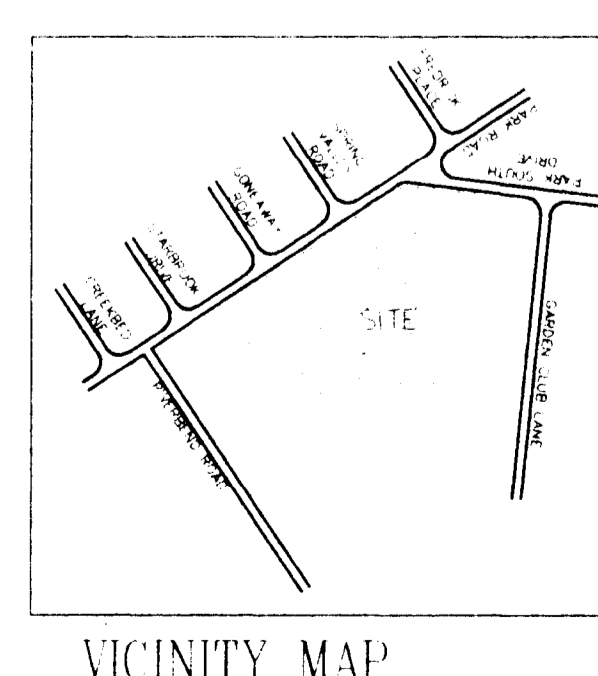
B. DEVELOPMENT REQUIREMENTS

(1) **General Provisions.** At a minimum, all development standards established under the City of Charlotte Zoning Ordinance (the "Ordinance") for the Institutional District shall be followed. In addition, development shall be governed by these Development Standards and the Technical Data Sheet.

The building configurations, placements and sizes as well as the locations of streets and parking areas shown on the Schematic Site Plan (Sheet RZ-2), are schematic in nature and may be altered or modified during design development and construction document phases within the maximum building limit lines established on the Technical Data Sheet.

(2) **Setbacks, Side Yards and Rear Yards.** All buildings constructed within the Site shall satisfy or exceed the setback, rear yard and side yard requirements established under the Ordinance for the Institutional District. A 50 foot setback will be established along Park Road and a 40 foot setback will be established along Park South Drive.

The setback areas along Park Road and Park South Drive will remain undisturbed, except for the clearing and grading that is necessary or desirable in connection with installing the proposed driveway entrance, entrance signage and landscaping at the driveway entrance, and except as noted otherwise in Paragraph (4) of Subsection B(4) of these Development Standards, or as noted otherwise on the Technical Data Sheet. Rear and side yards on the perimeter of the Site will be measured from the inside edge of the required buffer rather than from the external property line.



CURVE	RADIUS	ARC	CHORD	BEARING & DISTANCE
C1	742.94'	155.44'	N 16° 35' 36" E	155.16'
C2	213.18'	116.54'	N 43° 59' 12" E	115.09'

LINE	TABLE	LINE	TABLE	LINE	TABLE
L1	N 11° 27' 12" E	29.92'	L1	N 11° 27' 12" E	29.92'
L2	N 10° 36' 23" E	47.59'	L2	N 10° 36' 23" E	47.59'
L3	S 79° 23' 37" W	10.50'	L3	S 79° 23' 37" W	10.50'
L4	N 4° 49' 25" E	6.50'	L4	N 4° 49' 25" E	6.50'
L5	N 2° 28' 23" E	10.50'	L5	N 2° 28' 23" E	10.50'
L6	N 2° 28' 23" E	38.70'	L6	N 2° 28' 23" E	38.70'
L7	N 2° 28' 23" E	34.50'	L7	N 2° 28' 23" E	34.50'
L8	N 2° 28' 23" E	7.48'	L8	N 2° 28' 23" E	7.48'

- (3) **Buffer Areas.**
 - (a) Buffers established on the Technical Data Sheet shall conform to the standards of Section 12.302 of the Zoning Ordinance, subject, however to the provisions of Section 12.304 and to the provisions of Paragraphs (b) and (c) of this Subsection B(3).
 - (b) Buffer areas are to remain as open space, except to the extent necessary to accommodate access points, berms, walls, fences, grading, drainage pipes or channels, utility lines and facilities, pedestrian pathways and bicycle pathways. Sewer pipe installations may only cross undisturbed buffer areas at interior angles measured at property lines which are not less than 75 degrees and, to the extent possible, paths cleared for such utility lines shall be replaced with plant materials which are the same as or similar to the types of materials that are in place within the buffer area involved.
 - (c) In all buffer areas where existing trees and natural vegetation have been cleared to accommodate walls, berms, fences, grading, or the installation of utility construction, the cleared, unimproved areas will be landscaped as required by Section 12.302 of the Ordinance.
- (4) **Landscaped Areas and Screening.** Development shall, at a minimum, satisfy the provisions of the City of Charlotte Tree Ordinance.
 - (a) Screening shall conform with the standards and treatments specified in Section 12.303 of the Ordinance.
 - (b) Landscaped areas and sidewalks within the Site will be planned and improved in sequences which are keyed to development taking place on the Site.
 - (c) Dumpster and service areas will be screened from public streets and from adjacent properties in a manner which satisfies or exceeds the requirements of the Ordinance. Trash dumpsters and compactors will be screened with solid wood or fences not less than six feet in height on three sides, with gates on the fourth side. Trash collection for detached or attached cottage dwelling units will be performed by a private trash collection company, OR, THE CITY OF CHARLOTTE, from separate trash containers located at each unit.
 - (d) The Petitioner may remove vines, underbrush, small trees (2 inches in caliper or less) and shrubs within any portion of the 50 foot landscaped area established along Park Road and the 40 foot landscaped area established along Park South Drive for the purpose of improving the overall appearance of these naturally vegetated areas.
- (5) **Access Point.** Vehicular access to the Site will be limited to the one access shown on the Technical Data Sheet and will be located in the general area depicted. The configuration and ultimate location of this access point within the Site are subject to any minor modifications required to accommodate final site and architectural construction plans and designs and are further subject to approval by the City of Charlotte Department of Transportation.
- (6) **Lighting.**
 - (a) A uniform lighting system will be employed throughout the Site. All lighting within the Site except street lights which may be erected along Park Road and Park South Drive shall be designed such that direct illumination does not extend past exterior property lines of the Site. Consideration will be given to the impact of lighting both within and without the perimeter of the Site. Items for consideration will include intensity, cutoff angles, color, energy efficiency and shielding of sources of light, the intent being to eliminate glare towards adjacent streets and properties.
 - (b) Lighting fixtures will not exceed 15 feet in height within 200 feet of any abutting residential property, and will not exceed 25 feet in height within the remaining portions of the Site, except for street lights along Park Road and Park South Drive.
- (7) **Parking.**
 - (a) The parking spaces depicted on the Schematic Site Plan may vary but in all events will be sufficient to satisfy the minimum off-street parking standards established under the Ordinance, including Sections 12.203(1) and 12.203(2) of the Ordinance.
 - (b) No parking will be permitted within buffer areas, landscaped areas, or setback areas.
 - (c) Subject to the provisions of Paragraphs (a) and (b) above, parking will be permitted inside and outside of building envelopes.
- (8) **Signs.** A uniform signage and graphics system will be employed throughout the Site which shall comply with the provisions of Section 13 of the Ordinance.
- (9) **Fire Protection.** Adequate fire protection in the form of fire hydrants will be provided to the City of Charlotte Fire Marshal's specifications. Fire hydrants shall be located within 750 feet, as the fire truck travels, of the most remote and accessible point of all buildings. Plans for each building will be submitted to the City of Charlotte Fire Marshal's office for approval before construction of that particular building commences.
- (10) **Design Standards.**
 - (a) Landscaped islands will be provided in parking areas to avoid large expanses of pavement. These islands will satisfy the provisions of Section 12.208 of the Ordinance, and will be located such that no more than three parking spaces will be placed side by side without a planting island between spaces.
 - (b) Multi-unit buildings will not exceed five stories above no more than one story of parking, and in no case will building heights exceed 95 feet (68 feet maximum to eave plus 27 feet maximum for roof). Detached and attached cottage dwelling units will not exceed one story or 30 feet in height (11 feet maximum to eave plus 19 feet maximum for roof). The health care assisted living building will not exceed two stories or 55 feet in height (25 feet maximum to eave plus 30 feet maximum for roof). The clubhouse building will not exceed three stories or 70 feet in height (43 feet maximum to eave plus 27 feet maximum for roof). Maximum building heights do not include chimneys, cupolas, balustrades or similar elements.
 - (c) All parking will be screened from public streets in accordance with the requirements of the Ordinance.
 - (d) Cottage dwelling units will have a minimum front setback of 15 feet from private streets, and a minimum side setback of 10 feet from private streets (where building entrances do not occur).
 - (e) The Site will be subject to the requirements of Section 9.303 (19) of the Ordinance.
 - (f) Minimum building separation will be 10 feet between detached cottage dwelling units.
- (11) **Storm Drainage.**
 - (a) Storm water runoff from the Site will be managed through proven techniques which satisfy the standards imposed by the City of Charlotte Engineering Department.
 - (b) The entire Site will be subject to the 2-year and 10-year storm water detention requirements set forth in the Charlotte-Mecklenburg Storm Water Design Manual and, storm water detention will be provided that will also prevent an increase in flow rates during 25-year and 100-year storm events over flow rates which would be produced by development of the Site under its current R-3 zoning classification.
- C. STREETS AND DRIVEWAYS**
 - (1) Roadway improvements at the driveway leading into the Site will be made in accordance with the requirements of the City of Charlotte Engineering Department and the Charlotte Department of Transportation.
 - (2) A left turn lane with sufficient stacking distance leading from Park South Drive into the Site will be provided by the Petitioner, at its expense, and will be installed prior to the issuance of any permanent certificate of occupancy.
- D. AMENDMENTS**

Future amendments to this Technical Data Sheet and the Development Standards may be applied for by the then Owner or Owners of the Site in accordance with the provisions of Chapter 6 of the Ordinance.
- E. BINDING EFFECT OF THE REZONING APPLICATION**
 - (1) If this Rezoning Petition is approved, the development program established under these Development Standards, the Technical Data Sheet and other supportive documents shall, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioner and subsequent owners of the Site and their respective heirs, devisees, personal representatives, successors in interest and assigns.
 - (2) The terms "Petitioner", or "Owners", as used in this Technical Data Sheet and the accompanying Development Standards, shall be deemed to include the respective heirs, devisees, personal representatives, successors in interest and assigns of the Owner or Owners of the Site from time to time.

APPROVED BY CITY COUNCIL
 DATE September 16, 1996

DPR
 DESIGN-PLANNING-RESEARCH

DPR ASSOCIATES, INC.
 Landscape Architects
 2016 E. 10th Street
 Charlotte, NC 28204

DATE: 4/17/96
 PROJECT #: 96018.0 (DPR)
 RZ-1 of 2

REV DATE	DESCRIPTION
1 8/14/96	PER PLANN.
	COMM FOR
	PUB HEARING

The Cypress
 ARCHITECTURE
 LANDPLANNING INTERIOR DESIGN
 COPYRIGHT © THOMPSON HANCOCK WHITE & ASSOCIATES, INC.

PETITIONER: CYPRESS OF CHARLOTTE, LLC PETITION NO. 96-61

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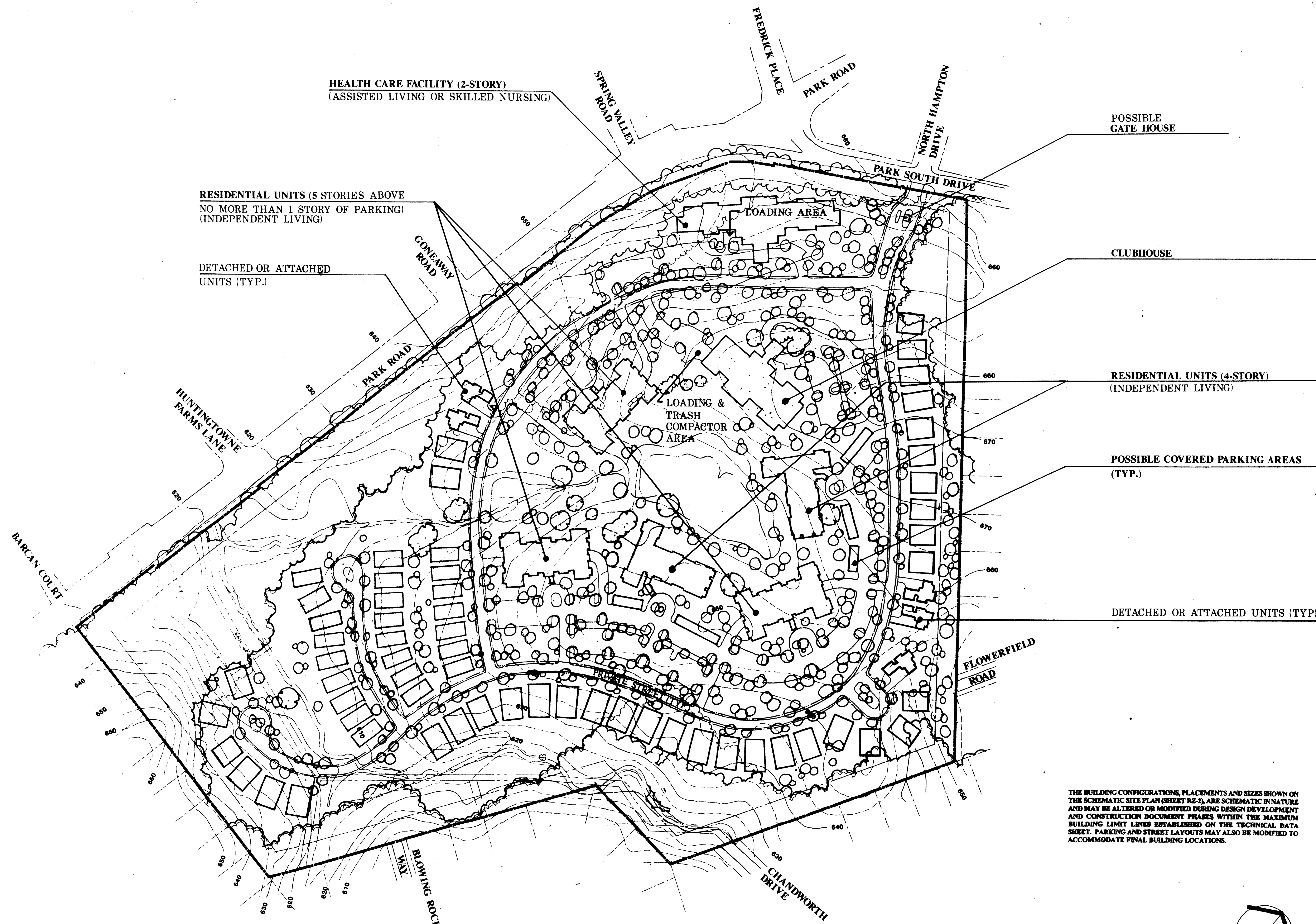
4055 ROSWELL ROAD, NE
 ATLANTA, GEORGIA 30342
 TELEPHONE: (404) 252-8040
 FAX: (404) 252-8064

TECHNICAL DATA SHEET

DATE: 4/17/96
 PROJECT #: 96018.0 (DPR)
 RZ-1 of 2

AS FOR PUBLIC HEARING

DATE: 4/17/96
 BY: MARTIN R. OKAMTON, JR.
 DPR JOB NO. 96018



THE BUILDING CONFIGURATIONS, PLACEMENTS AND SIZES SHOWN ON THE SCHEMATIC SITE PLAN (SHEET RZ-2), ARE SCHEMATIC IN NATURE AND MAY BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION DOCUMENT PHASES WITHIN THE MAXIMUM BUILDING LIMIT LINES ESTABLISHED ON THE TECHNICAL DATA SHEET. PARKING AND STREET LAYOUTS MAY ALSO BE MODIFIED TO ACCOMMODATE FINAL BUILDING LOCATIONS.

AS FOR PUBLIC HEARING

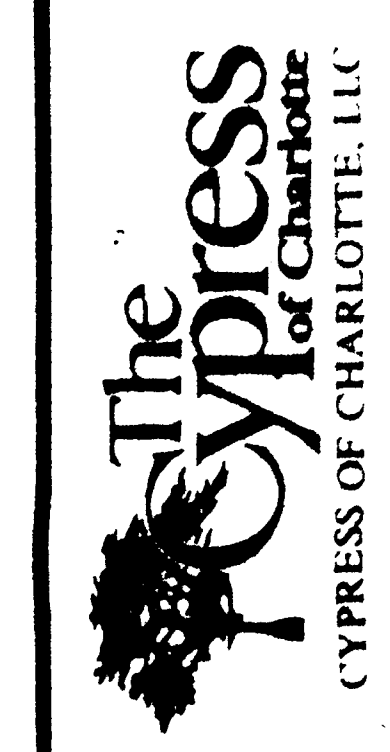
CHARLOTTE-MECKLENBURG
PLANNING COMMISSION
INTER-OFFICE COMMUNICATION

DATE: February 26, 1999
TO: Robert Brandon, Zoning Administrator
FROM: Martin R. Cranston, Jr., Planning Director

SUBJECT: Administrative Approval for Petition No. 96-61 by The Cypress of Charlotte, LLC tax parcel 179-082-31.

Attached is a revised conditional plan for the above mentioned project to reflect that trash pickup for the cottage units can be provided by rollout service through the City of Charlotte Sanitation Department. Since this change is minor I am administratively approving this revised plan. Please use this revised plan when evaluating requests for building permits and certificates of occupancy.

PETITION NO. 96-61



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D Sheet Title
SCHEMATIC
SITE PLAN

Date: 4/17/96
Project #: 9601

RZ-2