

RESIDENTIAL MIXED USE VILLAGE

EXISTING CONCORD MILLS MALL
(1,700,000 SQUARE FEET RETAIL)

RETAIL MIXED USE VILLAGE

OFFICE MIXED USE VILLAGE

TRANSIT STOP 1

TRANSIT STOP 2

Existing Zoning: R-3
 Proposed Zoning: CC
 Total Site Area: 348.1 Acres +/-
 Proposed Uses: Retail Mixed Use Village - 700,000 Square Feet Retail - 79 Acres +/-
 Office Mixed Use Village - 1,500,000 Square Feet Office - 73 Acres +/-
 Residential Mixed Use Village - 2400 Dwelling Units* - 198 Acres +/-
 - 30,000 Square Feet Support Retail

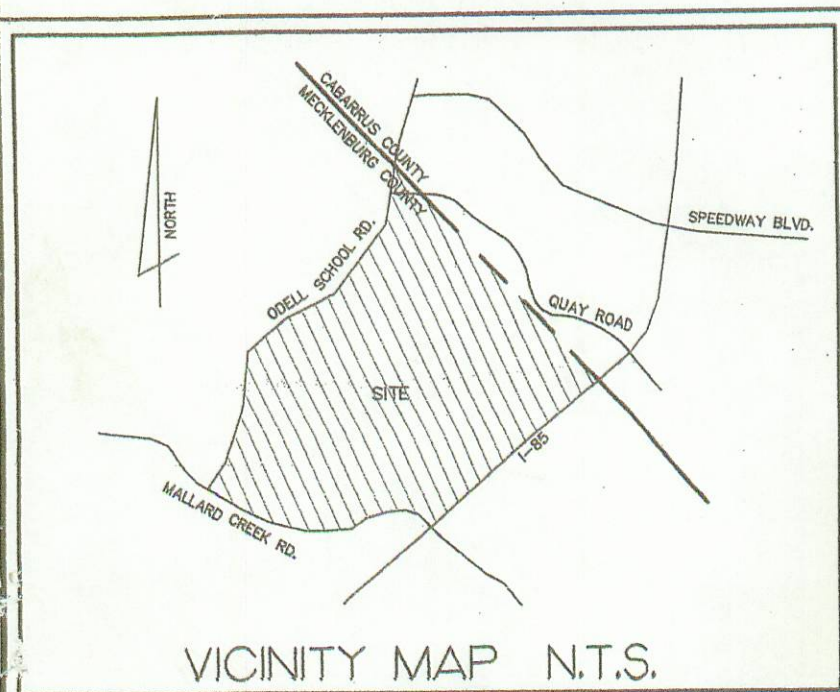
Note: All area calculations are approximate and are subject to change based on final design layout.

APPROVED BY COUNTY COMMISSION
 DATE 2/8/00

- RETAIL
- OFFICE
- HIGH DENSITY RESIDENTIAL UP TO 28 DUA
- MEDIUM DENSITY RESIDENTIAL UP TO 17 DUA
- SINGLE FAMILY UP TO 6 DUA
- TRANSIT STOP
- COMMUNITY SERVICE (DAY CARE, ETC.)
- ELEMENTARY SCHOOL SITE
- PUBLIC AREAS (PARKS, COMMUNITY OPEN SPACE, ETC.)

FOR ILLUSTRATIVE PURPOSES, THE VILLAGES STRADDLE THE COUNTY LINE. PETITION SITE DATA, USES AND SQUARE FOOTAGES ARE CALCULATED ONLY FOR MECKLENBURG COUNTY

ADDITIONAL 20' OF PROPOSED RIGHT-OF-WAY 50' FROM CENTERLINE SHALL BE DEDICATED PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS. PETITIONER SHALL ALSO INSTALL A 5' SIDEWALK WITH AN 8' PLANTING STRIP ALONG MALLARD CREEK ROAD.

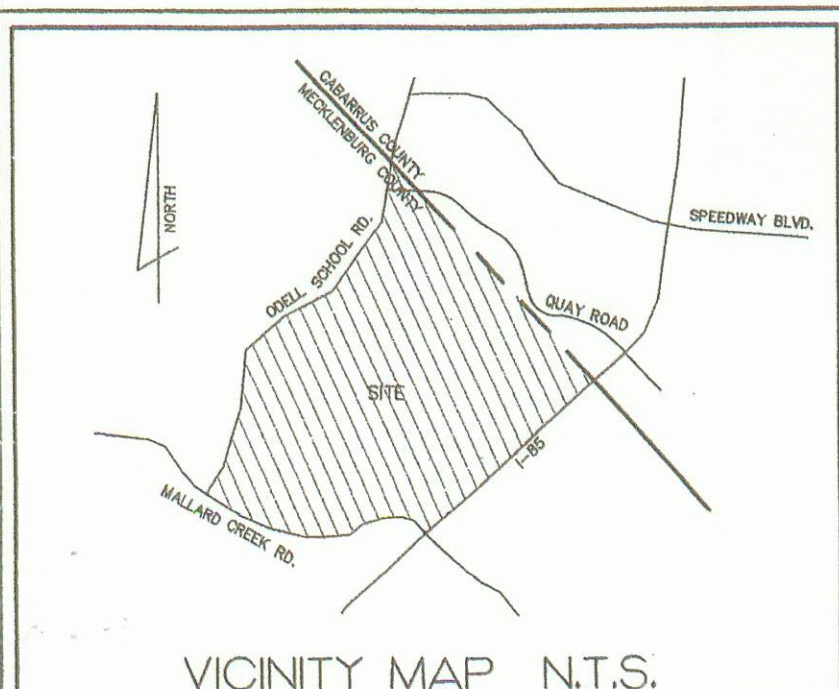
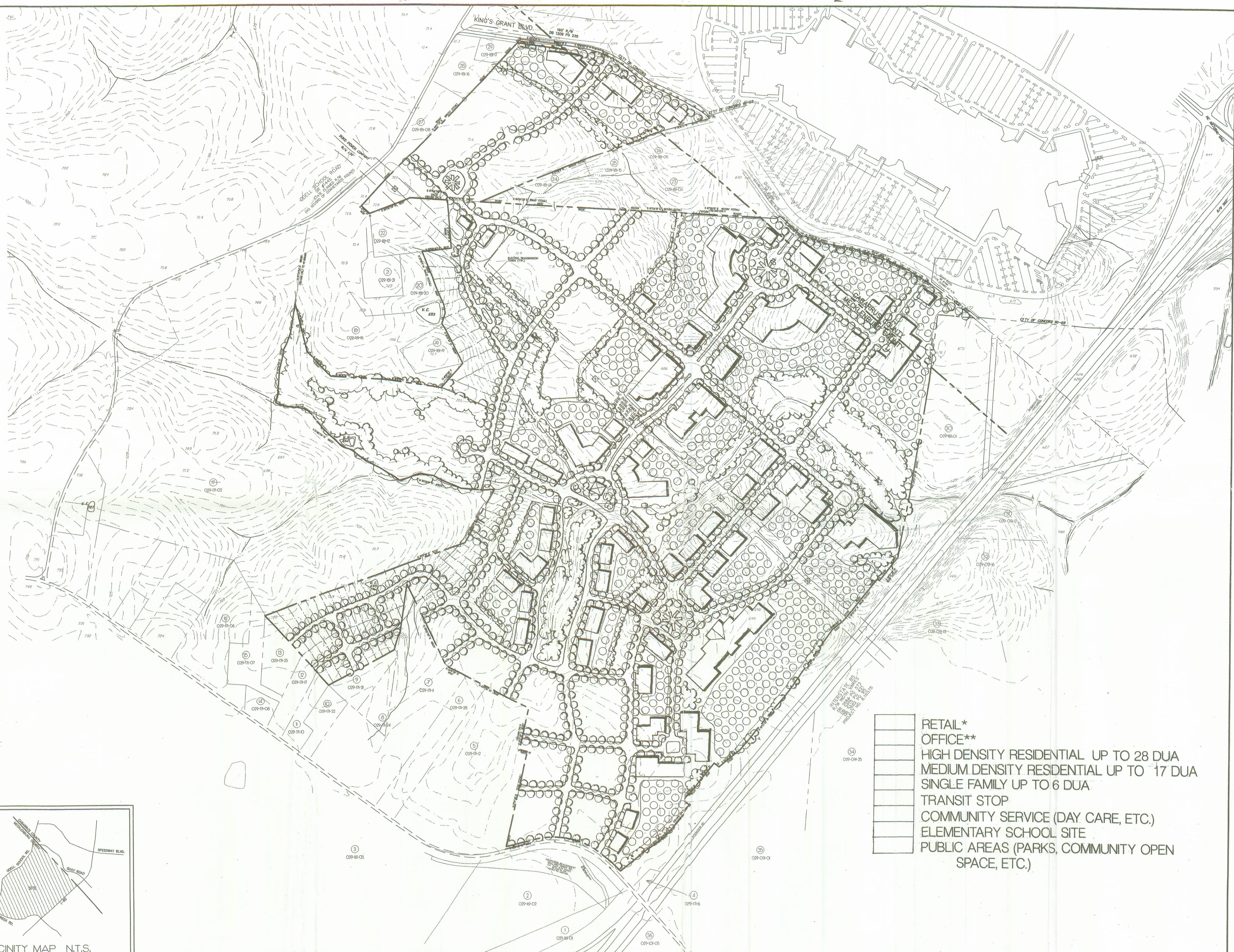


REZONING PLAN FOR
KINGS GRANT
 MECKLENBURG COUNTY, NORTH CAROLINA
CONCORD DEVELOPMENT GROUP, LLC
 SMITH TOWER, SUITE 420
 5555 HIGHWAY 29
 HARRISBURG, NC 28075
 PHONE: (704) 455-5411 FAX: (704) 455-9011

DESIGN RESOURCE GROUP, PA
 Landscape Architecture • Civil Engineering
 Land Planning
 500 East Boulevard
 Suite 102
 Charlotte, NC 28203
 Ph.: (704) 343-0608
 Fax: (704) 358-3093

PETITION # 99-050
 2-4-00
TECHNICAL DATA SHEET
 FOR PUBLIC HEARING

Scale: 1" = 300'	Date: 26 OCTOBER 1998
Revisions:	
1) 11/11/98	REVISED PARCEL LINES & CIRCULATION
2) 02/04/99	PER PC COMMENTS
3) 12 April 1999	
4) 2 JUNE 1999	
5) 20 JULY 1999	
6) 10 JANUARY 2000	
Sheet 1	of 4



- RETAIL *
- OFFICE **
- HIGH DENSITY RESIDENTIAL UP TO 28 DUA
- MEDIUM DENSITY RESIDENTIAL UP TO 17 DUA
- SINGLE FAMILY UP TO 6 DUA
- TRANSIT STOP
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- ELEMENTARY SCHOOL SITE
- PUBLIC AREAS (PARKS, COMMUNITY OPEN SPACE, ETC.)

REZONING PLAN FOR
KINGS GRANT
 MECKLENBURG COUNTY, NORTH CAROLINA
CONCORD DEVELOPMENT GROUP, LLC

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PETITION # 99-05

SKETCH PLAN

FOR PUBLIC HEARING



Scale: 1" = 300'
 Date: 2 JUNE 1999
 Revisions:

DEVELOPMENT STANDARDS FOR THE KINGS GRANT MIXED-USE DEVELOPMENT REZONING PETITION NO. 99-05

PART I

Commitment

Development of the real estate identified on the accompanying Technical Data Sheet prepared by Design Resource Group, PA on October 26, 1998 as amended on 4 June 1999, consisting of 348.8 acres, more or less (the "Site") and its individual components will be governed by these Development Standards, the Technical Data Sheet and all applicable provisions of the Mecklenburg County Zoning Ordinance (the "Ordinance").

PART II

Statement of Intent

It is the intention of the Petitioner to create within the Site three interconnected, pedestrian and transit friendly mixed-use villages to be known as Kings Grant offering a variety of shopping, employment and residential opportunities.

PART III

Permitted Development Within the Site

Section 1. RETAIL MIXED USE VILLAGE

The Retail Mixed Use Village encompasses an area of 79 acres, more or less, and is generally located south of the Cabarrus County line and west of that portion of the Office Mixed Use Village as generally depicted on the Technical Data Sheet.

- (a) The Retail Mixed Use Village may be used for any uses (including accessory uses) which are prescribed by right under the Ordinance.
- (b) The total floor area (including any permitted accessory use) which may be developed within the Retail Mixed Use Village may not exceed 670,000 square feet.
- (c) A maximum of two out parcels will be allowed within the Retail Mixed Use Village as a mixed use component, so long as the maximum number of residential units for the entire project does not exceed 2400 dwelling units or the maximum amount of office square footage for the entire site does not exceed 1,500,000 square feet.

Section 2. OFFICE MIXED USE VILLAGE

The Office Mixed Use Village encompasses an area of 73 acres, more or less, and is generally located between I-85 and the Retail Component and north of Mallard Creek Road.

- (a) The Office Mixed Use Village may be used for any general office uses and permitted accessory uses which are prescribed by right under the Ordinance.
- (b) The total floor area of general office uses (including any permitted accessory use which may be developed within the Office Mixed Use Village) may not exceed 1,500,000 square feet.
- (c) Residential units may be incorporated into mixed use buildings so long as the maximum number of residential units for the entire site shall not exceed 2400 dwelling units.
- (d) Support Retail may be incorporated as a mixed use component in the Office Mixed Use Village so long as it is limited to the ground floor only and so long as the maximum square footage of retail for the entire site does not exceed 700,000 square feet.

Section 3. RESIDENTIAL MIXED USE VILLAGE

The Residential Mixed Use Village encompasses an area of 198 acres, more or less, and is generally located north of Mallard Creek Road and west and south of the Retail and Office Mixed Use Villages.

- (a) The total number of dwelling units which may be constructed within the Residential Mixed Use Village may not exceed 2400 units.
- (b) Up to 30,000 square feet of support retail uses, except fast food, service station/convenience stores and motels/hotels, may be developed within the Residential mixed use village center areas which are generally depicted on the Technical Data Sheet.
- (c) The Residential Mixed Use Village may be devoted to any residential use (including any accessory use) which is permitted by right in the Commercial Center Development (CC) Zoning District under the Ordinance.
- (d) A variety of housing types and densities will be developed within the Residential Mixed Use Village and within the areas and at the densities depicted and described on the Technical Data Sheet.
- (e) At least 25 acres within the Residential Mixed Use Village will be devoted to the development of a minimum of 60 detached single family units.

Section 4. SCHOOL SITE

- (a) The Petitioner agrees to dedicate to Mecklenburg County a 20 acre site, more or less, for use as an elementary school within the general area depicted on the Technical Data Sheet. The precise location and size of the elementary school site will be determined by the Mecklenburg County Board of Commissioners after giving due consideration to the preferences of representatives from the School Board.

- (b) The conveyance of the School Site will be made by the Petitioner upon terms, conditions and restrictions which address items of mutual concern, including but not limited to, the uses to which the site may be devoted, the precise location and size of the site, the design of each building proposed to be constructed on the site and the reservation of a possibility of reverter in favor of the Petitioner should the site not be devoted to school purposes.

- (c) Toward this end, the Petitioner agrees within two years after the date on which this Zoning Petition is initially approved to enter into and diligently pursue negotiations with the Mecklenburg County Board of Commissioners leading toward a contract under the terms of which the Petitioner would agree to donate the School Site upon mutually acceptable terms and conditions.

- (d) The Petitioner's deed may reserve easements over all or any portion of the school site for drainage, sanitary sewers, water lines, telephone lines, power lines, cable television lines, gas lines and such other utility purposes as it may deem necessary or desirable for the orderly development of the Site.

- (e) The Petitioner's commitment with respect to the School Site is expressly conditioned upon approval of this Rezoning Petition.

- (f) If the Petitioner and Mecklenburg County have not executed a binding contract which provides for the dedication by the Petitioner of the School Site to Mecklenburg County on a closing date which is no later than sixty (60) days after the date on which the contract is executed and which contains terms and conditions mutually acceptable to the parties within ten (10) years after the date on which this Rezoning Petition was initially approved, the Petitioner shall be fully released from its commitment to Mecklenburg County with respect to the school site and may thereafter deal with such site as freely as if such commitment had never been made to Mecklenburg County.

- (g) If for any reason the School Site is not conveyed to Mecklenburg County (or to its nominee) within seven years and sixty (60) days after the date on which this Rezoning Petition shall have been initially approved, such site may be developed for low density residential uses (up to 6 DUA) as determined by the Petitioner.

Section 5. PEDESTRIAN AND OPEN SPACE COMPONENT

- (a) Each building or dwelling unit within the site shall be connected via a minimum 4' wide sidewalk to a public sidewalk system to be established along both sides of streets. Location of these connector sidewalks are discretionary and may parallel the location of parking lots or cross them, but may not run continuously within parking and vehicular circulation areas.

The street sidewalks will be at least 5' wide and separated from the street with a planting strip at least 6' wide, except along the Commercial Town street and the Boulevard street where sidewalks shall be 6' wide and planting strips shall be 8' wide. All as generally depicted on the street cross sections under Section 4 of these conditions.

- (b) The Open Space System, as generally depicted on the Technical Data Sheet encompasses an area of 15 acres more or less and will be used for the purposes of providing alternative transportation for pedestrians and bicyclist to provide alternative cross-connectivity, as well as to preserve existing natural stream beds and open space.

- (c) Development parcels within the site which adjoin the Open Space System must provide pedestrian access to the open space system via sidewalks which are at least 4 feet in width.

- (d) The Pedestrian and Bicycle pathway within the Open Space System shall consist of an eight foot (8') paved trail of either concrete or asphalt. Development of the pathway system will be implemented in such a manner as to minimize stream crossings and disturbances.
- (e) The Open Space System and the SWIM Buffers shall be delineated on the record plat(s) for all affected parcels within the entire development. (See SWIM Buffers compliance commitment under Part VI, Section 4.)

PART IV Transportation Commitments

The Petitioner and each future owner shall be obligated to satisfy the following transportation commitments:

Section 1. TRANSIT STUDY AND COMMITMENTS

The Petitioner has caused a transit-oriented development study to be made at its expense, which assessed the feasibility of extending transit services from the University area to this Site and made recommendations with respect to ways in which the proposed development might be designed so as to facilitate the use, circulation and operation of the public transit system.

The site will have three transit stops which will be located in the general areas as depicted on the Technical Data Sheet. The timing for the completion of each of these transit stops will be in accordance with the following schedule in Part V of these conditions. Transit Stop 1 will be completed as a part of Phase One of the Roadway Improvements. Transit Stop 2 will be completed upon or before the completion of more than 500,000 square feet of office development. Transit Stop 3 is smaller in nature and optional. It may be completed at any time if the need is established for it by the City of Charlotte Public Transportation Department.

The Petitioner shall coordinate with the City of Charlotte Public Transportation Department to develop the designs of these transit stops.

Each of these transit stops will, at a minimum, satisfy the following specifications:

- (a) Transit stops 1 and 2 will accommodate up to 6 to 8 transit vehicles, as necessary. Stops will be configured to handle both shuttle vehicles and coaches up to 40 feet in length.
- (b) Transit stop 3 (optional) shall accommodate up to 2 to 3 transit vehicles as necessary. The stop will be configured to handle both shuttle vehicles and coaches up to 40' in length as necessary.
- (c) Transit stops will be located near the retail centers to provide convenient shopping and amenities for transit patrons.
- (d) Public spaces adjacent to the transit stop will be sufficiently sized to accommodate transit shelters, sidewalks, benches, ticket vending, information kiosks, bicycle racks and other passenger amenities, and such amenities will be installed as warranted.
- (e) Sidewalks, walking trails and bike paths will be planned to provide connectivity to each transit.
- (f) Shelters shall be pedestrian size, roofed open air structures.

Section 2. DEDICATION OF RIGHT-OF-WAY

- (a) The North Carolina Department of Transportation is currently considering providing for a half diamond interchange with I-85 which would be located between the I-485 interchange and Exit 49 (Speedway Boulevard), the exact location of which has not been pinned down. One of the locations under consideration involves a portion of this Site. If this is the ultimate location selected for this improvement, Petitioner agrees to dedicate all right-of-way falling within its Site which is necessary to accommodate a typical half diamond interchange with ramp termini located 400 feet from the centerline of I-85. If the interchange is located within this site in a location not depicted generally on the Technical Data Sheet the Petitioner reserves the right to reconfigure proposed "Spine Road, Roads, and Parcels so as to accommodate this new alignment.

- (b) The Petitioner shall dedicate 20' additional right of way (50' from centerline) along Mallard Creek Road as part of platting the spine road.

Section 3. (deleted, see Part V, Transportation Commitments)

Section 4. INTERIOR ROADWAYS

The Petitioner will construct a network of interconnecting streets and blocks that facilitate a pedestrian and transit friendly environment, generally as depicted on the Technical Data Sheet. The layout of the street network may be revised and adjusted as long as the basic layout principles remain the same and the revised layout functions as well.

Streets generally depicted on the Technical Data Sheet shall meet the cross section layout and design standards as shown in Part VI, Section 4, and will also meet the pavement cross section standards as specified in the City or County Development Standards. They shall be offered for public dedication.

In addition, the City of Charlotte Public Transportation Department shall be given the opportunity to review and comment on the design/construction documents of these streets as it pertains to the transit friendly aspects, such as radii at intersections, curve lengths, etc.

- (1) Dead end streets shall be prohibited unless specifically indicated on the Technical Data Sheet or prior written consent is provided by the Planning Director. In all events, each dead end street must terminate in a courtyard, open space type turn around.

- (c) Block length in the Retail or Office Mixed Use Villages may not exceed 1000' more or less. Block length in the Residential Mixed Use Village may not exceed 600', smaller blocks are preferable.

- (d) The construction of interior streets will be in sequences keyed to the various phases of development taking place on the Site.

- (e) All streets constructed within the site, either public or private, must satisfy the design criteria established below. The petitioner may eliminate parallel parking on one side of the street at their discretion.

PART V Phasing of Development

The Petitioner agrees to develop Kings Grant in accordance with the phasing schedule set forth below:

Phase 1

Authorized Phase 1 development includes up to 300,000 square feet of retail development, up to 300,000 square feet of office development, and up to 300 attached dwelling units. The developer may adjust this Phase 1 development program (i.e. increase the size of one land use type and decrease that of another) as long as the traffic impacts are neutral. Evaluation of traffic impact neutrality will be subject to review and approval by the Mecklenburg County Engineering Department.

The Petitioner may proceed with Phase 1 building construction at its discretion. Furthermore, building occupancy permits may be issued for the entire residential portion of Phase 1 development (300 attached dwelling units). However, no occupancy permits may be issued for any non-residential Phase 1 development unless and until all of the following Phase 1 roadway improvements have been completed or complied with:

- Access A (eastern driveway on Quay Road), including a traffic signal [if (and only if) its installation is warranted], an eastbound exclusive right-turn lane, westbound exclusive left-turn lane, and northbound exclusive left-turn lane. Installation of Access B includes the realignment of the existing Kings Grant Boulevard/Quay Road intersection so as to create a through movement from Odell School Road to I-85.
- Access E (eastern driveway on Mallard Creek Road) and Spine Road connecting Accesses A and E. The Spine Road is to be constructed as a two-lane road initially with a four-lane ultimate section (to occur by the end of Phase 2). Access E will be constructed with an eastbound exclusive left-turn lane, westbound exclusive right-turn lane, and southbound exclusive left-turn lane.
- Transit stop 1 within the development.
- Concord Mills Boulevard/Odell School Road intersection: Install a traffic signal [if (and only if) its installation is warranted] and add one westbound exclusive left-turn lane (providing dual westbound left-turn lanes).
- Odell School Road from Concord Mills Boulevard to Quay Road: Add one southbound lane.
- Quay Road/Odell School Road intersection: Install a traffic signal [if (and only if) its installation is warranted] and add one southbound exclusive left-turn lane (providing dual southbound left-turn lanes).
- Quay Road from Odell School Road to Access B: Add an eastbound through lane.
- Odell School Road/Mallard Creek Road intersection: Install a traffic signal [if (and only if) its installation is warranted], install one southbound right-turn lane, and one westbound right-turn lane, install dual eastbound left-turn lanes.

- Mallard Creek Road/Salome Church Road intersection: Install a traffic signal if (and only if) its installation is warranted.
- The Petitioner has paid One Million Dollars (\$1,000,000) to the Mecklenburg County Engineering Department or its designee to be used for offsite roadway improvements to Odell School Road (between Quay Road and Mallard Creek Road) and/or to Mallard Creek Road (between the proposed Odell School Road and Salome Church Road).

Phase 1 roadway improvements are depicted in Figure 1 attached hereto. Funding for this construction will be the responsibility of the developer.

Phase 2

In addition to Phase 1 development, authorized Phase 2 development includes up to 400,000 square feet of retail development, up to 550,000 square feet of office development, and up to 900 attached dwelling units. Total authorized development upon completion of Phase 2 includes 700,000 square feet of retail development, 850,000 square feet of office development, and 1,200 attached dwelling units. The developer may adjust this Phase 2 development program (i.e. increase the size of one land use type and decrease that of another) as long as the traffic impacts are neutral. Evaluation of traffic impact neutrality will be subject to review and approval by the Mecklenburg County Engineering Department.

Following completion of all Phase 1 roadway improvements, building permits may be issued for Phase 2 development. However, building occupancy permits may not be issued for more than thirty percent of Phase 2 development (based on trip generation) unless and until the Petitioner has paid one million dollars (\$1,000,000) to Mecklenburg County for offsite roadway improvements [to Odell School Road (between Quay Road and the proposed Mallard Creek Road) and/or to Mallard Creek Road (between the proposed Odell School Road and Salome Church Road)] and all of the following Phase 2 roadway improvements have been completed:

- Construction of the I-85/Quay Road bridge, including the widening of Quay Road to four lanes from the I-85/Quay Road bridge to Access B. Funding for this construction will be the responsibility of the developer.
- Installation of a connector road from the Speedway Boulevard/Weddington Road intersection to Old Holland Road. Funding for this construction will be the responsibility of the developer.
- Access A/Quay Road intersection: Install a traffic signal [if (and only if) its installation is warranted], add a westbound exclusive left-turn lane, add an eastbound right-turn lane, and add a northbound exclusive right-turn lane. Funding for this construction will be the responsibility of the developer.
- Access B/Quay Road intersection: Install a northbound right-turn lane. Funding for this construction will be the responsibility of the developer.
- Odell School Road/Quay Road intersection: Add one northbound through lane from Quay Road to Concord Mills Boulevard by Developer, subject to NCDOT's willingness to condemn the necessary right-of-way (if required), with the cost of such condemnation to be paid by the Developer.
- Access E/Mallard Creek Road intersection: Add a traffic signal if (and only if) its installation is warranted. Funding for this construction will be the responsibility of the developer.
- Mallard Creek Road/Salome Church Road intersection: Realign to allow Salome Church Road through operation install a southbound right-turn lane and an eastbound left-turn lane.

Phase 2 roadway improvements are depicted in Figure 2 attached hereto.

Phase 3

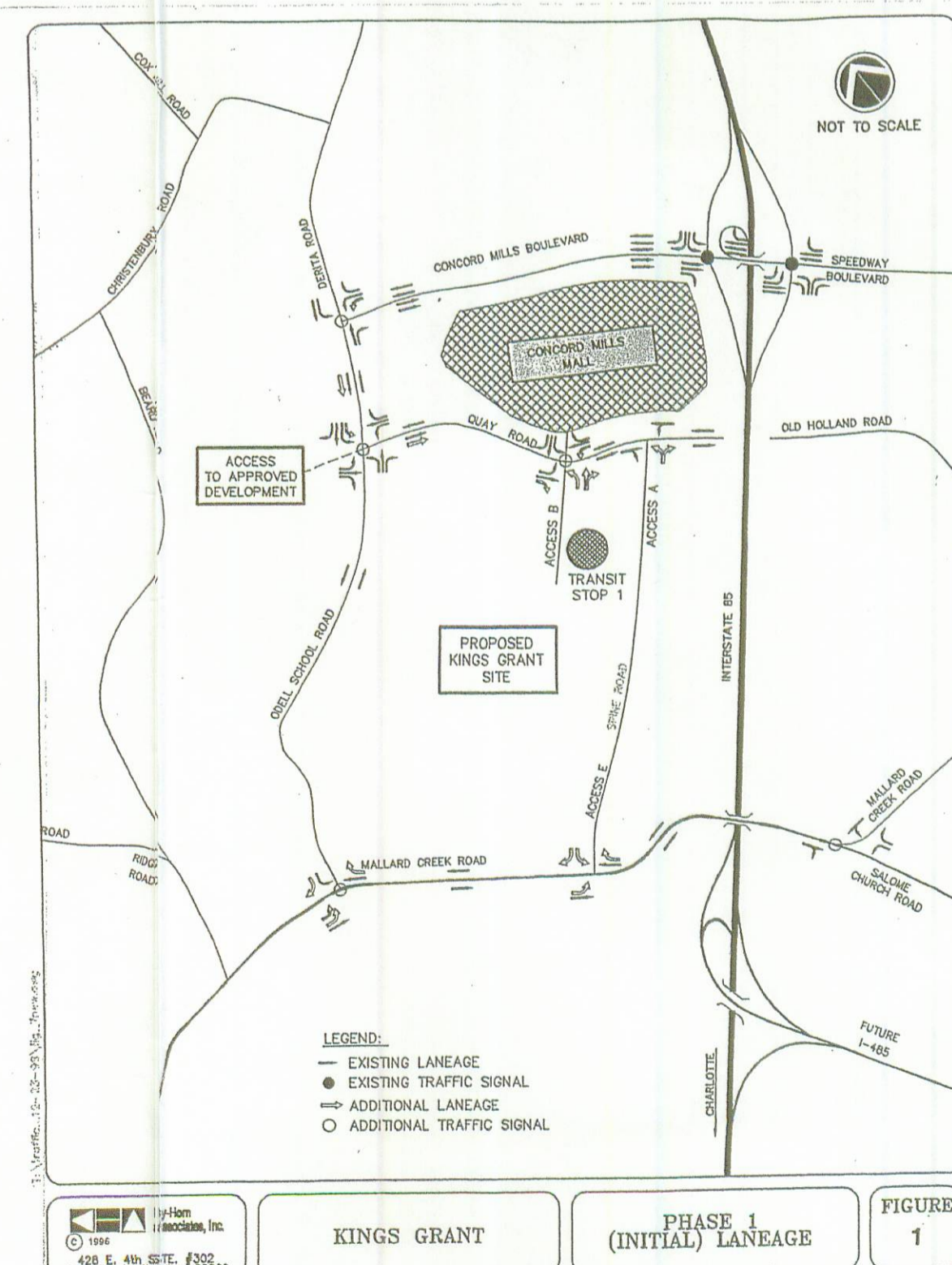
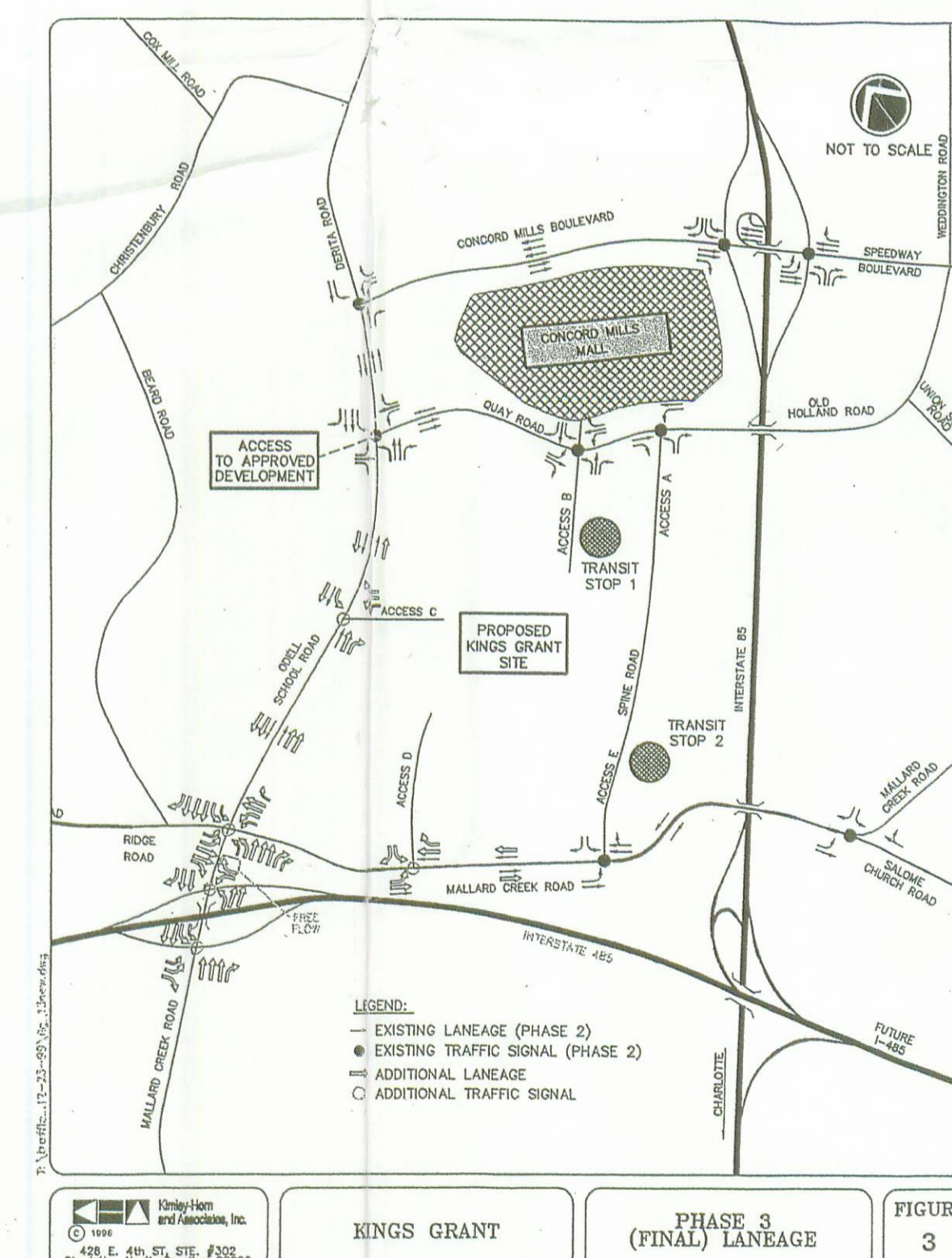
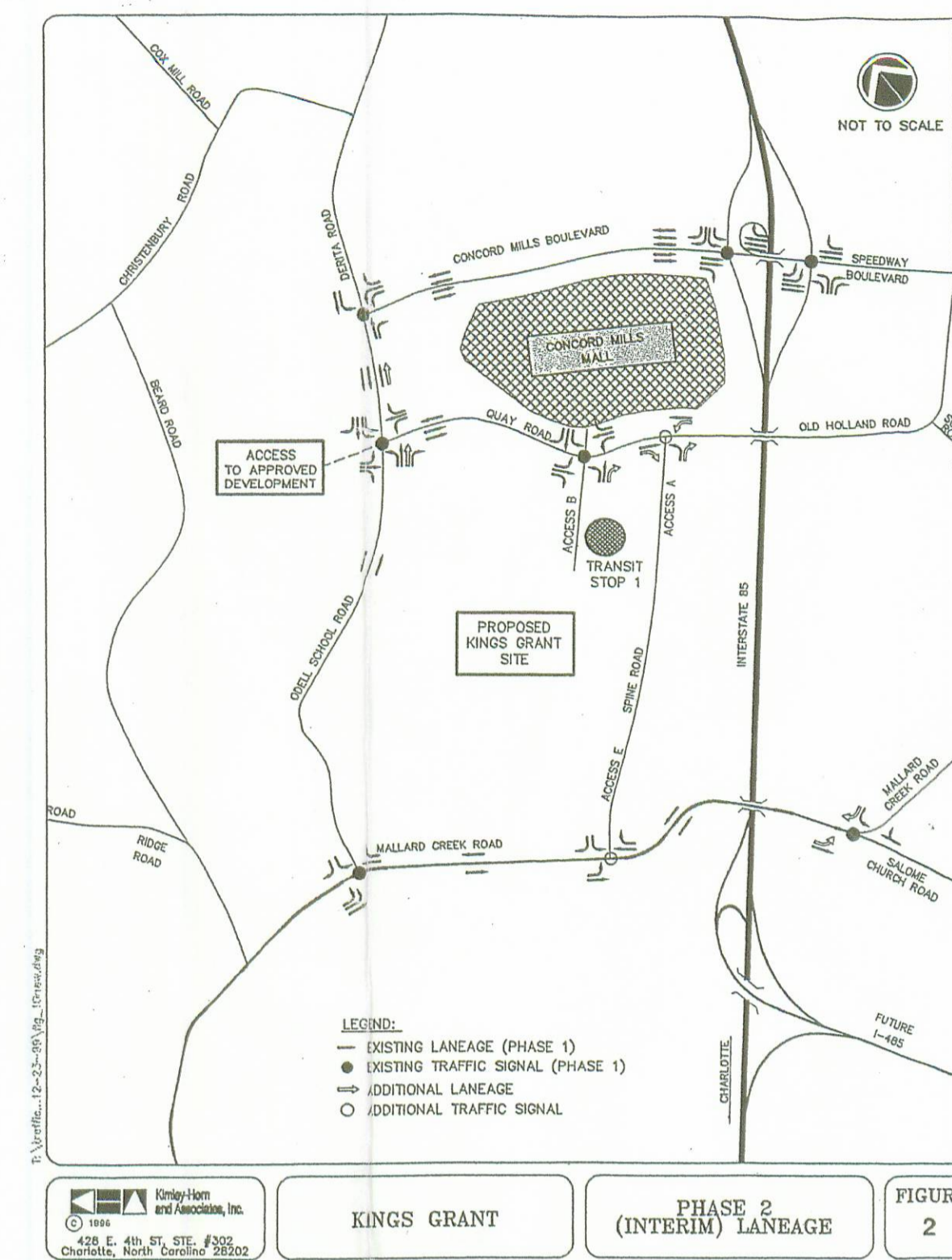
In addition to Phase 1 development and Phase 2 development, authorized Phase 3 development includes up to 650,000 square feet of office development, up to 120 detached dwelling units, and up to 1,149 attached dwelling units. Total authorized development upon completion of Phase 3 includes 700,000 square feet of retail development, 1,500,000 square feet of office development, 2,349 attached dwelling units, and 120 detached dwelling units. The developer may adjust this Phase 3 development program (i.e. increase the size of one land use type and decrease that of another) as long as the traffic impacts are neutral. Evaluation of traffic impact neutrality will be subject to review and approval by the Mecklenburg County Engineering Department.

Building occupancy permits may be issued for Phase 3 development when contracts have been let for construction of all of the following Phase 3 roadway improvements:

- I-485/Mallard Creek Road Interchange.
- Realigned Mallard Creek Road/Odell School Road/Ridge Road intersection.
- Installation of Access C and Access D. Funding for this construction will be the responsibility of the developer.
- Transit stop 2 within the development. Funding for this construction will be the responsibility of the developer.

However, building occupancy permits may not be issued for more than thirty percent of Phase 3 development (based on trip generation) or for more than the entire residential portion of Phase 3 development unless and until the Phase 3 roadway improvements listed above have been completed.

Phase 3 roadway improvements are depicted in Figure 3 attached hereto.



**REZONING PLAN FOR
KINGS GRANT
MECKLENBURG COUNTY, NORTH CAROLINA
CONCORD DEVELOPMENT GROUP, LLC**

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Scale: _____
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Revisions:
 1) 02/04/99 per PC
 COMMENTS
 2) 17 March 1999 - Sec. 3.c.
 3) 12 April 1999
 4) 2 June 1999
 5) 20 July 1999
 6) 10 January 2000
 7) 02 FEBRUARY 2000

Sheet **3** of **4**

PART VI
General Design Guidelines for all Villages

Section 1. BUILDING ENTRANCES AND FAÇADE ARTICULATION

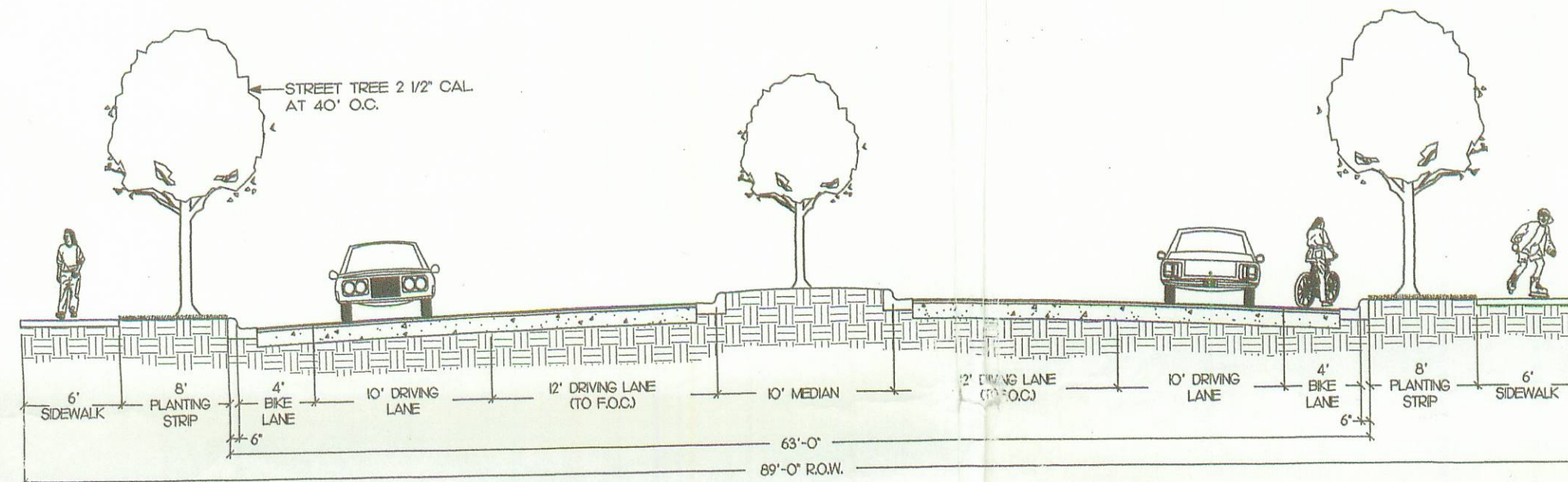
- (a) All principal buildings must be oriented to and have their main entrances from a street or plaza/open space. Secondary entries shall be allowed from interior blocks or parking lots.
- (b) Elevations facing the street shall be varied and articulated with a combination of street level windows, arcades, bays, multiple entrances, balconies, etc., the intent being to ensure that no building elevation fronting a street consist of a blank unarticulated wall.

Section 2. YARD RESTRICTIONS

- (a) Interior setbacks may be reduced per section 11.405 (7) of the Ordinance.
- (b) In order to facilitate a pedestrian and transit friendly environment, the petitioner shall be allowed a minimum front yard of 12' in the Residential and Office Mixed Use Villages and shall be allowed 12' front yard requirement in the Retail Mixed Use Village.
- (c) A minimum 25 foot rear yard will be maintained in all villages.
- (d) Unless otherwise provided on the Technical Data Sheet or in the Ordinance, in every instance the side yard, setback and rear yard requirements governing development in the C.C. District Ordinance taking place on the exterior boundary of the Site will be satisfied.

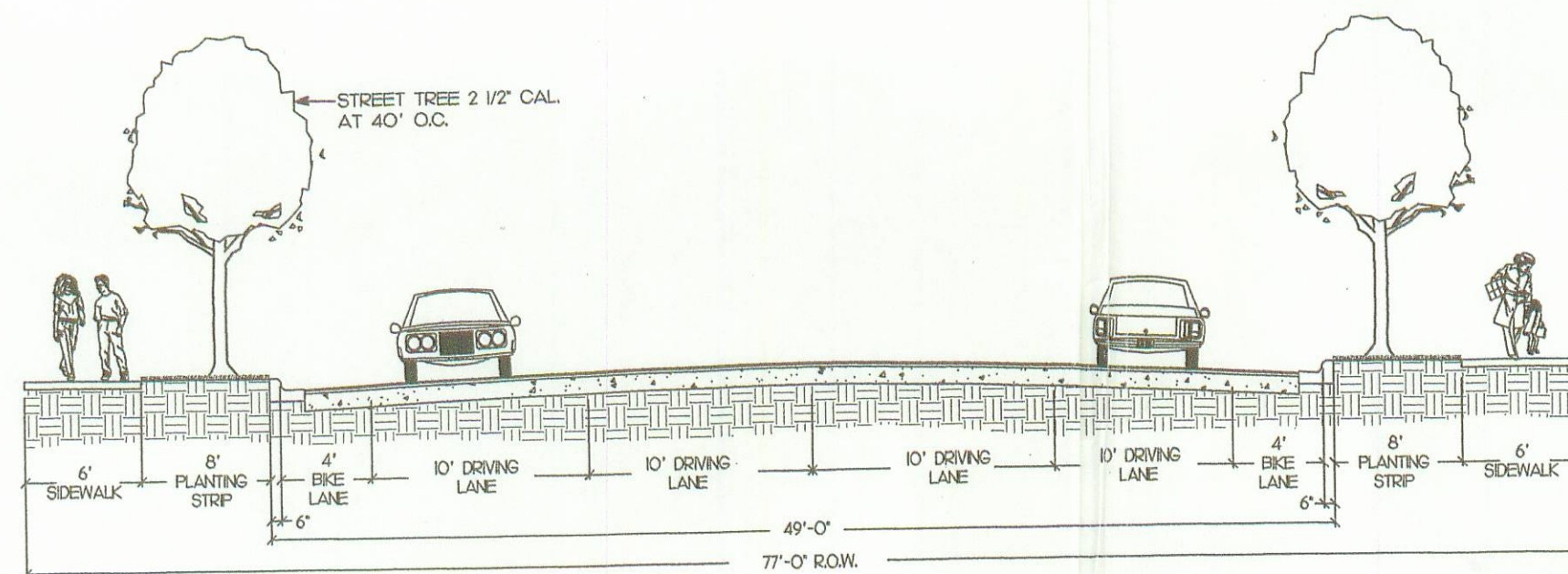
Section 4. STREETS

- (a) The number of exterior vehicular access points to the Site shall be limited to the number depicted on this Technical Data Sheet. The number of interior access points shall be established through State/County driveway permitted process.
- (b) The placement and configuration of each access point are subject to any minor modifications required to accommodate final site and architectural construction plans and designs and to any adjustments required for approval by the County Engineering Department and the North Carolina Department of Transportation.
- (c) On street parallel parking shall be allowed.
- (d) All public and private streets shall have stand up curb and gutter and shall meet the applicable City or County standards.
- (e) A coordinated Streetscape Tree Planting Plan shall be developed for the entire site prior to approval of any construction documents.
- (f) Sidewalks on Boulevard and Commercial Town Streets shall be a minimum of 6' wide.



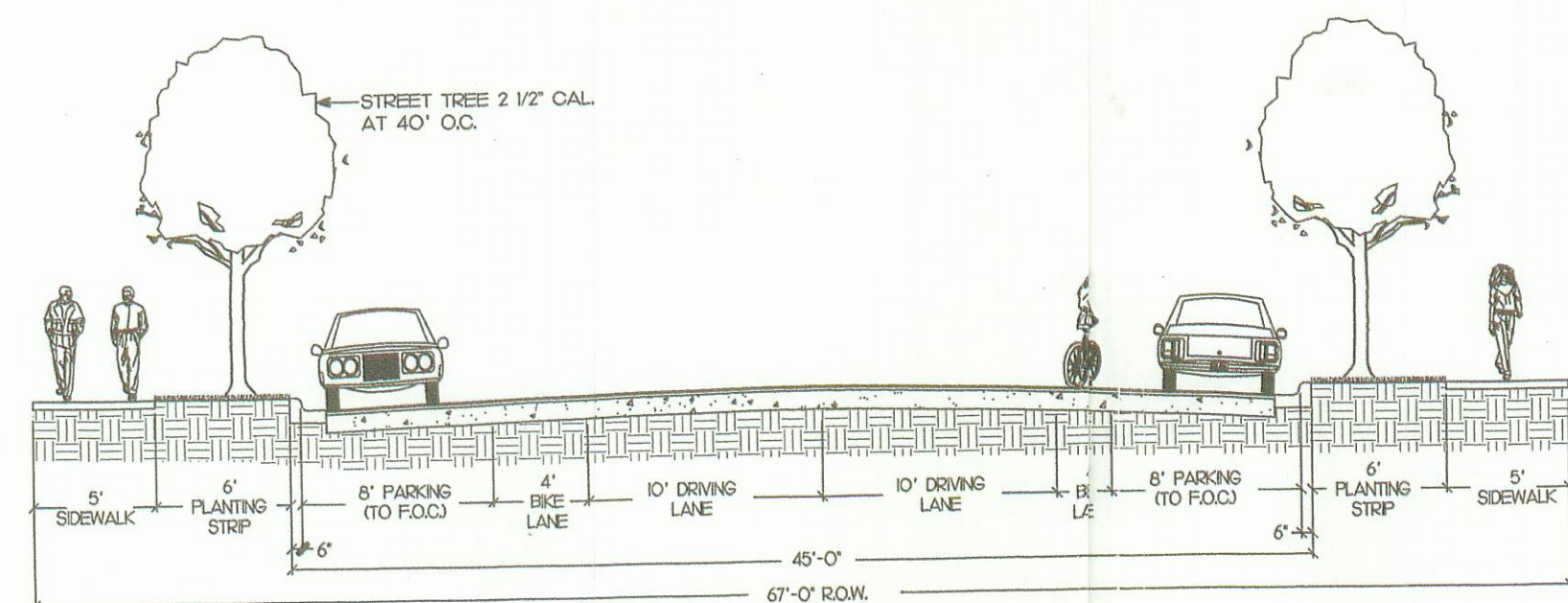
** ROW MAY BE INCREASED TO PROVIDE DESIGNATED ON-STREET PARKING LANE, 7' TO 8' IN WIDTH.

A 91' R.O.W. - BOULEVARD DETAIL
NOT TO SCALE

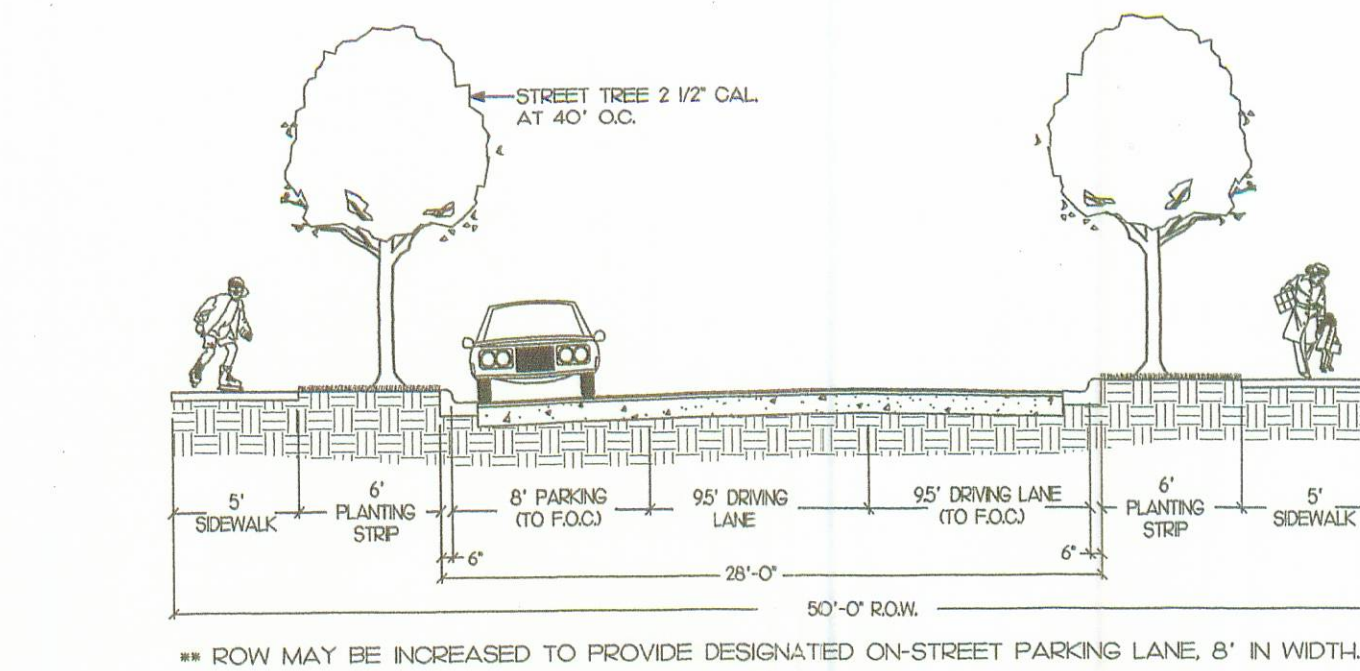


** ROW MAY BE INCREASED TO PROVIDE DESIGNATED ON-STREET PARKING LANE, 8' IN WIDTH.
*** 2 LANE VERSION OMIT 10' DRIVING LANES.

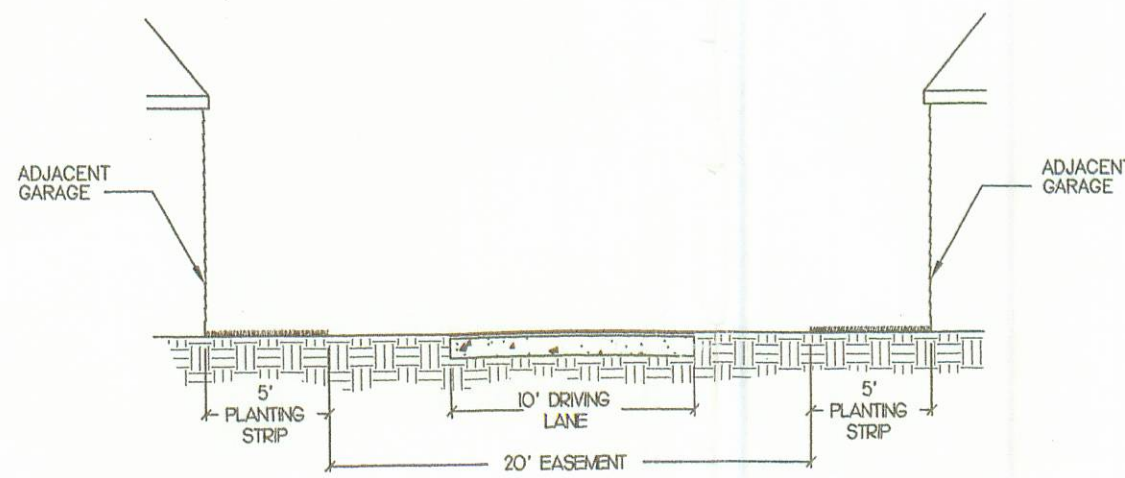
B 77' R.O.W. - COMMERCIAL TOWN STREET DETAIL
NOT TO SCALE



C 67' R.O.W. - RESIDENTIAL TOWN STREET DETAIL
NOT TO SCALE



D 50' R.O.W. - NEIGHBORHOOD STREET DETAIL
NOT TO SCALE



E ALLEY DETAIL
NOT TO SCALE

Section 5. PARKING

- (a) Parking lots or garages can not be adjacent to a square or Open Space area, as generally depicted on the Technical Data Sheet.
- (b) A parking lot shall be located at the rear or side of a building. If located at the side, screening shall be provided at the lot line by landscaping or decorative walls or fences.
- (c) The use of alleys shall be encouraged and front load garages shall be kept to a minimum. Where front load garages are used the garage must set back 5' from the principal structure.
- (d) In mixed use areas, shared parking plans may be allowed at the discretion of the Zoning Administrator.

Section 3. SIGNS AND GRAPHICS

A master signage and graphics system will be adopted and, at a minimum, will satisfy the requirements of the Mecklenburg County Sign Ordinance.

- (a) All free standing retail signage or overall master plan identification signage shall be a maximum of 12' height. All office signage shall be a maximum of 7' height.
- (b) All freestanding building and tenant identification signs and graphics installed will be complementary in scale and appearance to the structures which they identify.
- (c) No sign may be mounted on the roof of any structure.
- (d) Public information signs and graphics will be easily understood and complementary in scale and appearance.

Section 4. STORM WATER MANAGEMENT

- (a) A Master Storm Water Management Concept Plan shall be developed and approved prior to the approval of any construction documents. Storm water runoff from the Site will be managed through a variety of proven techniques which at a minimum satisfy the standards imposed by Mecklenburg County.
- (b) No detention ponds may be located within setback areas or buffer areas.
- (c) The petitioner will follow the requirements of the S.W.I.M Buffer Ordinance.

- (d) No filling or grading will be allowed in the Floodway Fringe, except as needed to accommodate community recreational needs, utility and road crossings.

Section 5. KEY ENTRYWAYS

Special attention will be given to the key entryways to the project. Landscape materials including street trees, shrubs and ground cover will establish continuity at each entryway, while creating a pleasant and harmonious interface with other site elements, such as signage, pedestrian circulation ways, lighting and adjacent buildings.

Section 6. BUFFER AREAS

- (a) Subject to the provisions of paragraph (e) below, exterior buffer areas will be established in accordance with the specifications provided on the Technical Data Sheet. Additional Buffer Areas will be provided when required by the Ordinance. All Buffer Areas will, at a minimum, conform to the standards of Sections 12.302 of the Ordinance; subject, however, to the provisions of Section 12.304 of the Ordinance.
- (b) Berms and grading may be installed or performed - and the width of the buffer may be reduced in those portions of Buffer Areas which are designated as "potential grading areas" on the Technical Data Sheet to the extent allowed by the Ordinance. When grade transition areas are needed within the Buffer, 25' from the exterior property line shall be left undisturbed. All other Buffer Areas are to remain substantially undisturbed, except to the extent necessary to accommodate access points, walls, fences, drainage pipes or channels, utility lines and facilities, pedestrian pathways and bicycle pathways.
- (c) In all undisturbed buffer areas where existing trees and natural vegetation have been cleared in order to accommodate pedestrian paths or the installation of utility lines, the cleared, unimproved areas will be landscaped as required by Section 12.302 of the Ordinance.

- (d) Utility installations may only cross buffer areas at interior angles measured at property lines which are not less than 75 degrees and, to the extent possible, paths cleared for such utility lines shall be replanted with plant materials which are the same as or similar to the types of materials that are in place within the buffer area involved.
- (e) If in the future adjoining zoning districts are changed to zoning districts which either do not require a buffer or only require a reduced buffer, then the buffer provided at the applicable edge depicted on the Technical Data Sheet may be adjusted in accordance with Table 12.302(a) and Table 12.302(b) of the Ordinance.

- (f) The Petitioner reserves the right to waive all interior buffer requirements.

Section 7. SCREENING

- (a) Screening shall conform with the standards and treatments specified in Section 12.303 of the Ordinance.
- (c) All roof mounted mechanical equipment will be screened from view.
- (d) Any dumpster's visible from a public street or from adjoining property will be screened with a solid-enclosure with gates.

Section 8. LIGHTING

- (a) All freestanding lighting fixtures (excluding public streetlights) installed within each Parcel will be uniform in design.
- (b) The maximum height of any freestanding lighting fixture (excluding public streetlights), including its base, within the Retail components shall not exceed 30 feet in height.
- (c) The maximum height of any freestanding lighting fixture (excluding public streetlights), including its base, within the Office and Residential Components shall not exceed 20 feet in height.
- (d) All free standing light fixtures and wall pack light fixtures installed within each Component other than streetlights erected along public streets shall be designed such that direct illumination does not extend past any exterior property line. Consideration will be given to the impact of lighting both within and without the perimeter of the Site. Items for consideration will include intensity, cutoff angles, color, energy efficiency and shielding of sources of light, the intent being to eliminate glare towards adjacent streets and properties.

Section 9. DESIGN REVIEW COMMITTEE

The Petitioner agrees to establish an Owners' Association. One of the duties of the Board of Directors of this association will be to establish a Design Review Committee. This committee shall have responsibility for adopting and implementing guidelines for all development taking place within the project other than within the Residential Component.

Key areas to be addressed by these guidelines will include landscaping, open space and signage criteria which incorporate the concepts which have been developed by the Petitioner's land planning consultant, Design Resource Group, PA. The Committee will consist of at least three members. The Petitioner further agrees to invite a member of the Charlotte-Mecklenburg Planning Commission's Staff to serve on the Committee.

PART VII
Amendments to Rezoning Plan

Future amendments to the Rezoning Plan, the Technical Data Sheet and these Development Standards involving one or more component Parcels may be applied for by the then Owner or Owners of the Parcel or Parcels involved in accordance with Chapter 6 of the Ordinance.

REZONING PLAN FOR
KINGS GRANT
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TYPICAL
 STREET
 CROSS
 SECTIONS

Scale:	NOT TO SCALE
Date:	26 OCTOBER 1998
Revisions:	
1)	02/04/99 per PC
COMMENTS	
2)	17 March 1999 - Sec. 3.c.
3)	12 April 1999
4)	2 June 1999
5)	20 July 1999
6)	10 January 2000