

VICINITY MAP
NTS

SITE DEVELOPMENT DATA

Site Acreage: +/-18.5 AC.

Tax Parcels: 091-111-01, 091-111-03, 091-111-04, 091-111-05, 091-111-06, 091-112-11, 091-112-40, 091-112-10, 091-111-05, 091-112-09 and 091-112-33

Proposed Zoning: TOD-M (CD)

Existing Zoning: I-1, I-2, and I-2 (CD) by Rezoning Petition No. 2009-006

Existing Uses: Industrial - Northeast Submarket

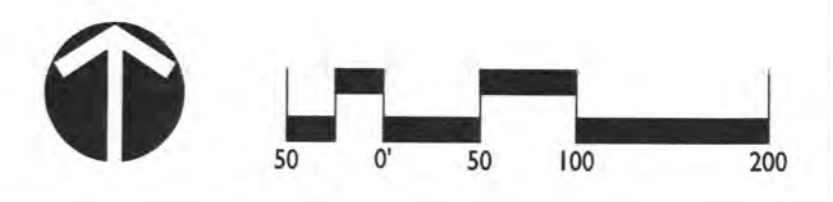
Proposed Uses: Uses permitted by right and under prescribed conditions together with accessory uses, as allowed in the TOD-M zoning district (as designated in Permitted Uses Section of development standards notes).

Residential Density: Per Zoning Ordinance (TOD-M)

APPROVED BY
CITY COUNCIL
JUL 18 2016

SITE LEGEND

- SITE ACCESS
- GREENWAY ACCESS
- ZONING LINE
- STREET NETWORK
- PEDESTRIAN NETWORK
- DEVELOPMENT AREA
- DEDICATED PROPERTY AS PART OF XCLT



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NODA GREENWAY SITE
REZONING PETITION No. 2016-79
FLYWHEEL GROUP
TECHNICAL DATA SHEET

REVISIONS:
1. 5/20/16 PER STAFF COMMENTS
2. 6/23/16 PER STAFF COMMENTS
3. 7/7/16 ZONING COMMITTEE COMMENTS

DATE: 07/07/16
DESIGNED BY: [blank]
DRAWN BY: AM
CHECKED BY: [blank]
G.C. BY: [blank]
SCALE: 1"=100'
PROJECT #: 4068-001

RZ-I

DEVELOPMENT STANDARDS

- DEVELOPMENT DATA TABLE**
Included on sheet RZ-1.
- GENERAL PROVISIONS**
 - These Development Standards form a part of the Rezoning Plan associated with the Rezoning Petition filed by The Flywheel Group (the "Petitioner") to accommodate a transit supportive mixed use development on that approximately 18.5 acre site located on the west side of Craighead Road between North Tryon Street and North Davidson Street, which site is more particularly depicted on the Rezoning Plan (hereinafter referred to as the "Site"). The Site is comprised of Tax Parcel Nos. 091-111-01, 091-111-03, 091-111-04, 091-111-05, 091-111-06, 091-111-07, 091-111-08, 091-111-09, 091-111-10, 091-111-05, 091-112-09 and 091-112-33.
 - The development of the Site will be governed by the Rezoning Plan, these Development Standards and the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Subject to the optional provisions set out below, the regulations established under the Ordinance for the TOD-M zoning district shall govern the development and use of the Site.
 - The development and uses depicted on the Rezoning Plan are schematic in nature and are intended to depict the general arrangement of uses and improvements on the Site. Accordingly, the ultimate layout, locations and sizes of the development and site elements depicted on the Rezoning Plan are graphic representations of the proposed development and site elements, and they may be altered or modified in accordance with the setback and yard requirements set forth on this Rezoning Plan and the development standards, provided, however, that any such alterations and modifications shall comply with Section 6.207. Future amendments to the Rezoning Plan and/or these Development Standards may be applied for by the then owner or owners of the Site in accordance with the provisions of Chapter 6 of the Ordinance. Alterations to the Rezoning Plan are subject to Section 6.207 of the Ordinance.
 - The parcels of land that comprise the Site may be recombined into one parcel at the option of the Petitioner or further subdivided.
 - The Site shall be considered to be a planned/unified development. Therefore, side and rear yards, buffers, building height separation requirements and other zoning standards shall not be required internally between improvements and uses on the Site. The Site shall be required to meet any applicable side and rear yard requirements and buffer requirements with respect to the exterior boundary of the Site.
 - Pursuant to Section 1.110 of the Ordinance and Section 160A-385.1 of the North Carolina General Statutes, the Rezoning Plan, if approved, shall be vested for a period of 5 years due to the size and phasing of the development, the level of investment, economic cycles and market conditions.
 - If this Rezoning Petition is approved, all conditions applicable to the use and development of the Site imposed under these Development Standards and the Rezoning Plan will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of Petitioner and the current and subsequent owners of the Site and their respective successors in interest and assigns.
 - Throughout these Development Standards, the term "Petitioner" shall be deemed to include the heirs, devisees, personal representatives, successors in interest, and assigns of Petitioner or the owner or owners of the Site from time to time who may be involved in any future development thereof.
 - Any reference to the Ordinance herein shall be deemed to refer to the requirements of the Ordinance in effect as of the date this Rezoning Petition is approved.

- OPTIONAL PROVISIONS**
 - The Petitioners seeks the optional provision to exceed the maximum allowed height of the TOD-M Ordinance for residential uses/buildings within Development Area 1 by 12 feet, and only for architectural elements such as towers and alike that may or may not contain heated area.

- PERMITTED USES**
 - The Site may be developed with residential and non-residential uses as permitted by right and under prescribed conditions in the TOD-M Zoning district together with accessory uses as allowed in the TOD-M zoning district.

- TRANSPORTATION**
 - Vehicular access shall be as generally depicted on the Rezoning Plan. The placement and configuration of the access points are subject to any minor modifications required by the Charlotte Department of Transportation ("CDOT") and/or the North Carolina Department of Transportation ("NCDOT").
 - Philemon Avenue is the PRIMARY Street and Craighead is the SECONDARY STREET. All others shall be considered secondary as well.
 - The alignment of the internal vehicular circulation areas and the driveways may be modified by the Petitioner to accommodate changes in traffic patterns, parking layouts and any adjustments required for approval by CDOT and/or NCDOT in accordance with applicable published standards.
 - The Petitioner will dedicate and convey 40 feet of right-of-way from the center line of Craighead Road as described below.

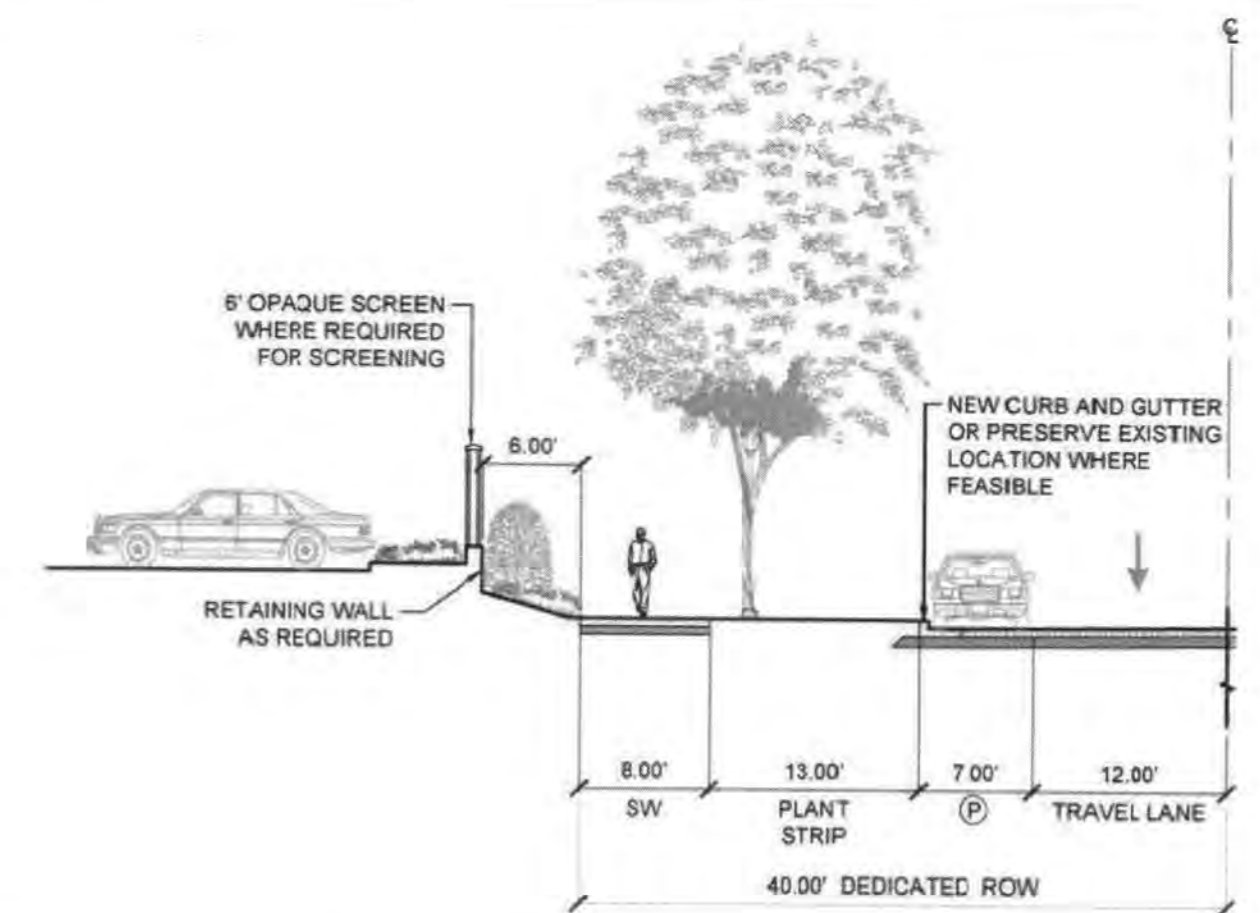
- ARCHITECTURAL AND DESIGN STANDARDS**
 - Entrances that face the trail for ground floor units shall be provided for all units viewable from the Cross Charlotte Trail.
 - Building entries: Ground floor entrances shall be provided for all residential units on Philemon Avenue. If ground floor unit individual entrances are not provided on other streets, a primary building entrance shall be provided at a minimum every 100'. On Philemon Street, a building entrance may be located at the corner, with additional entrances at a minimum of every 100' linear feet for non-residential uses.
 - Facades fronting on Philemon Street shall include more transparency on the ground floor than upper floors. 100% of the frontage on Philemon Avenue shall be either residential or non-residential leasable, usable space, with a minimum height of 12' from floor to floor for the first floor, and a minimum depth of 20' excluding public right of way, parking access drives, open space, and land environmentally undevelopable.
 - Units shall be raised an average of 24" above grade. Units shall be no more than 5' above grade and 2' below. Each entrance shall include a minimum of three of the following:
 - decorative pedestrian lighting/scones
 - architectural details carried through to upper stories
 - covered porches, canopies, awnings or sunshades
 - Archways
 - transom windows
 - terraced or raised planters that can be utilized as seat walls
 - common outdoor seating enhanced with specialty details, paving, landscaping, or water features
 - double doors (entrances should give an appearance of a front door orientation rather than a back patio design)
 - stoops or stairs

- ARCHITECTURAL AND DESIGN STANDARDS**
 - Pertaining to Townhomes: Pitched roofs, if provided, shall be symmetrically sloped no less than 5:12, except that roofs for porches and attached sheds may be no less than 2:12, unless a flat roof architectural style is employed. Usable porches and stoops shall form a predominate feature of the building design and be located on the front and/or side of the building. Usable front porches are covered and are at least 6 feet deep. Stoops and entry-level porches may be covered but not be enclosed. All corner/end units that face a public or private street should have a porch or stoop that wraps a portion of the front and side of the unit or provide blank wall provisions that limit the maximum blank wall expanse to 10ft on all building levels. Sidewalks should be provided to connect all residential entrances to sidewalks along public and private streets. Townhouse buildings should be limited to 5 individual units or less and ideally alternate the number individual units per building in adjacent buildings.
 - Blank walls shall be treated with both horizontal and vertical variations in wall planes.
 - Ground floor on all streets shall be taller than, and architecturally different than upper floors with more transparency than upper floors.
 - Buildings over 150' in length shall provide facade variations that visually separate the individual units. This can be accomplished through masses such as window arrangement and size variation, unit entrance design, roof variation, material changes, and/or offset wall planes.
 - The ground floor of parking structures viewable from public right of way shall be wrapped with active uses such as residential, office, and retail
 - No parking structure entrances shall be directly from Philemon Avenue. Driveway access off Philemon will be accepted.

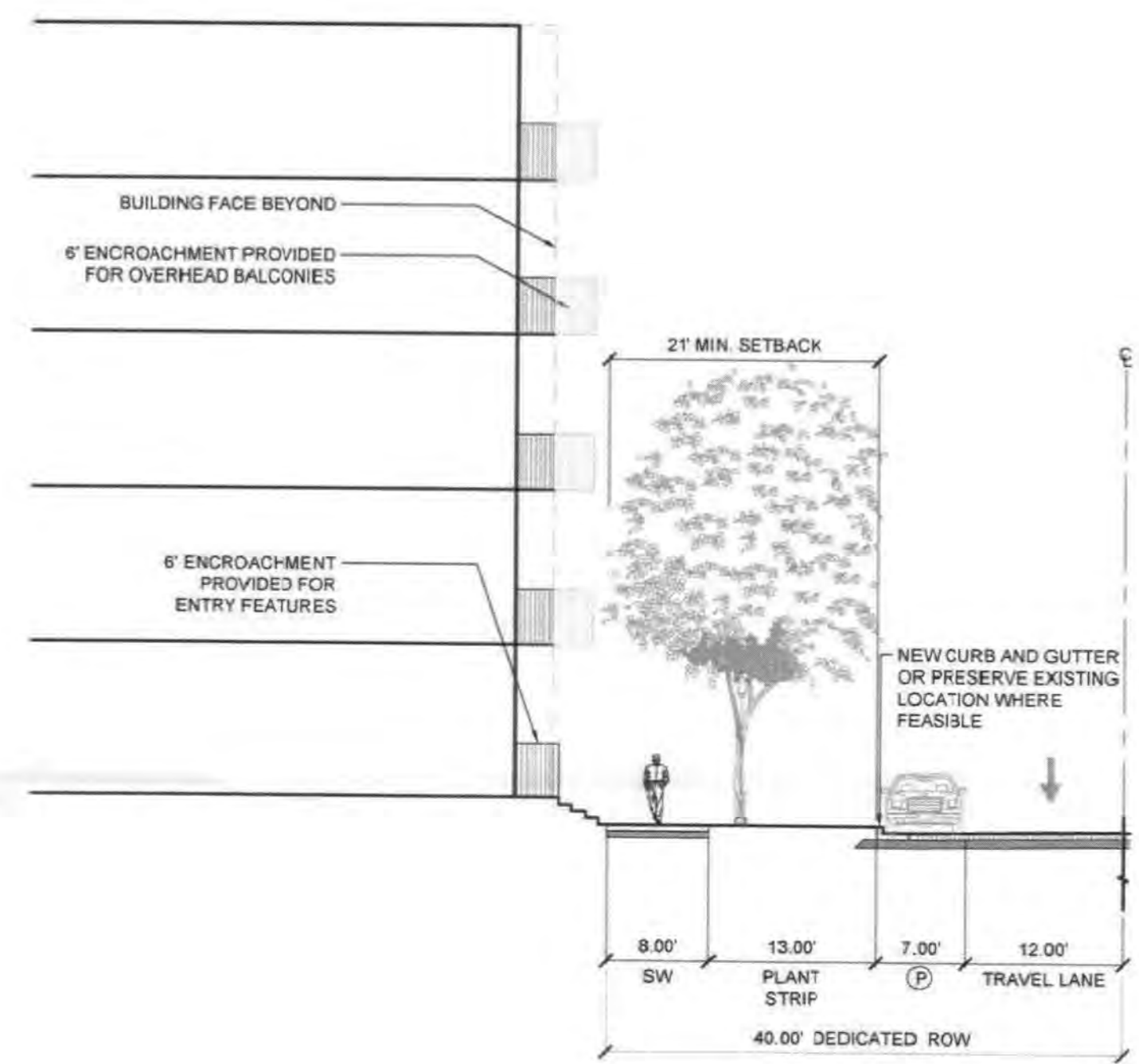
- STREETSCAPE AND LANDSCAPING**
 - Any ground floor commercial use with on-street parking shall have tree grates in lieu of a planting strip. Use of tree grates along Philemon Avenue streetscape shall be conditional based on coordination with the City of Charlotte.
 - Centerline of Philemon Ave has been adopted in accordance with the plan previously studied by Engineering and Property Management. The Petitioner will set the back of curb along each side of Philemon which will allow for a symmetric cross section containing a through lane, on-street parallel parking, 6' bike lane, 8' planting strip, and 8' sidewalk on each side of the centerline alignment. The Petitioner will make these improvements along Philemon Ave as development along Philemon Ave occurs. The Petitioner will be responsible for constructing improvements on the Site's side of the centerline Philemon Ave when sufficient pavement exists on the opposite side of the center line to accommodate a lane of traffic. If insufficient pavement width exists to accommodate a through lane on the opposite side of the center line of Philemon Ave the petitioner will improve both sides of Philemon Ave as indicated above.
 - The Petitioner will set the back of curb along the west side of Craighead which will allow for a through lane, on-street parallel parking, 6' bike lane, 8' planting strip, and 8' sidewalk. The future bike lane will be incorporated into the proposed planting strip.
 - Any dumpster and recycling areas located on the Site that are visible from the public street or from an adjoining parcel of land will be screened from view by solid-enclosures with grates.
- ENVIRONMENTAL FEATURES**
No optional provisions taken.
- PARKS, GREENWAYS AND OPEN SPACE**
 - Petitioner will coordinate with NECL team for the Cross Charlotte Trail (the "XCLT") during development and review of the site plan.
 - All buildings along the XCLT shall have a minimum 5' setback and include a transition from the sidewalk to the building. The building can include landscaping, outdoor dining, or seating
 - Provide direct connections to the XCLT from the development.
 - Provide pedestrian scaled lighting on connections leading to the XCLT.
 - Utilities shall be screened from the XCLT. This includes transformers, mechanical and electrical equipment, utility meters, dumpsters, and backflow preventers. Meter banks will also be screened from the public right-of-way.
 - No fencing shall be located between the building and the XCLT.
 - Public trail connections to be provided at no less than every 400'. Public trail connections shall be built to a minimum pavement width of 10 feet.
 - Petitioner shall install a minimum 35' wide cross section of the XCLT. The exact location of the XCLT within the development areas and in relationship to the Little Sugar Creek shall be determined during the permitting process. Petitioner shall grant to the city a minimum 35' wide easement to accommodate the relevant portions of the XCLT. The Developer shall not be responsible for maintenance of the XCLT. Petitioner shall install the XCLT within the Development Areas as each Development Area is developed, and dedicate and convey the 35' wide easement to the City prior to the issuance of the first certificate of occupancy.
 - The building edge along the XCLT will encroach into the previously disturbed SWIM Buffer. The preferred location of the building edge is an average of 33' north of this Buffer. Pending final location of the XCLT within the Mitigated Use and/or Stream Side zones of the Little Sugar Creek, the building will encroach a minimum of 22' to the north of this Buffer.
 - Petitioner agrees to obtain permits for and construct an 8 foot wide pedestrian pathway and a separated 12 foot wide two-way bicycle facility within the minimum 35' XCLT easement. The final alignment and cross section of the XCLT will be determined during permitting in coordination with the CDOT's Project Manager. Specific details for partnering between the City and the Developer for this facility will be determined during permitting.
 - Petitioner agrees to dedicate and convey permanent easements, and temporary easements along the site's Little Sugar Creek frontage. Easements will be dedicated and conveyed in fee simple, and all easement dedication and conveyance will occur prior to the issuance of the first certificate of occupancy for the site or when requested by the City, whenever occurs first. Easements will be dedicated and conveyed will be as follows:
 - Greenway/Trail easements equal to 35 feet
 - Rights-of-entry on an as-needed basis during construction in order to make tie-ins and/or if the City's project needs to construct Petitioner-requested improvements on the Petitioner's site.
 - The proposed greenway commitments will be made as part of each development phase that abuts the greenway and must be completed and approved before the first building certificate of occupancy for the development phase is issued.

3. Public trail connections to be provided at no less than every 400'. Public trail connections shall be built to a minimum pavement width of 10 feet.

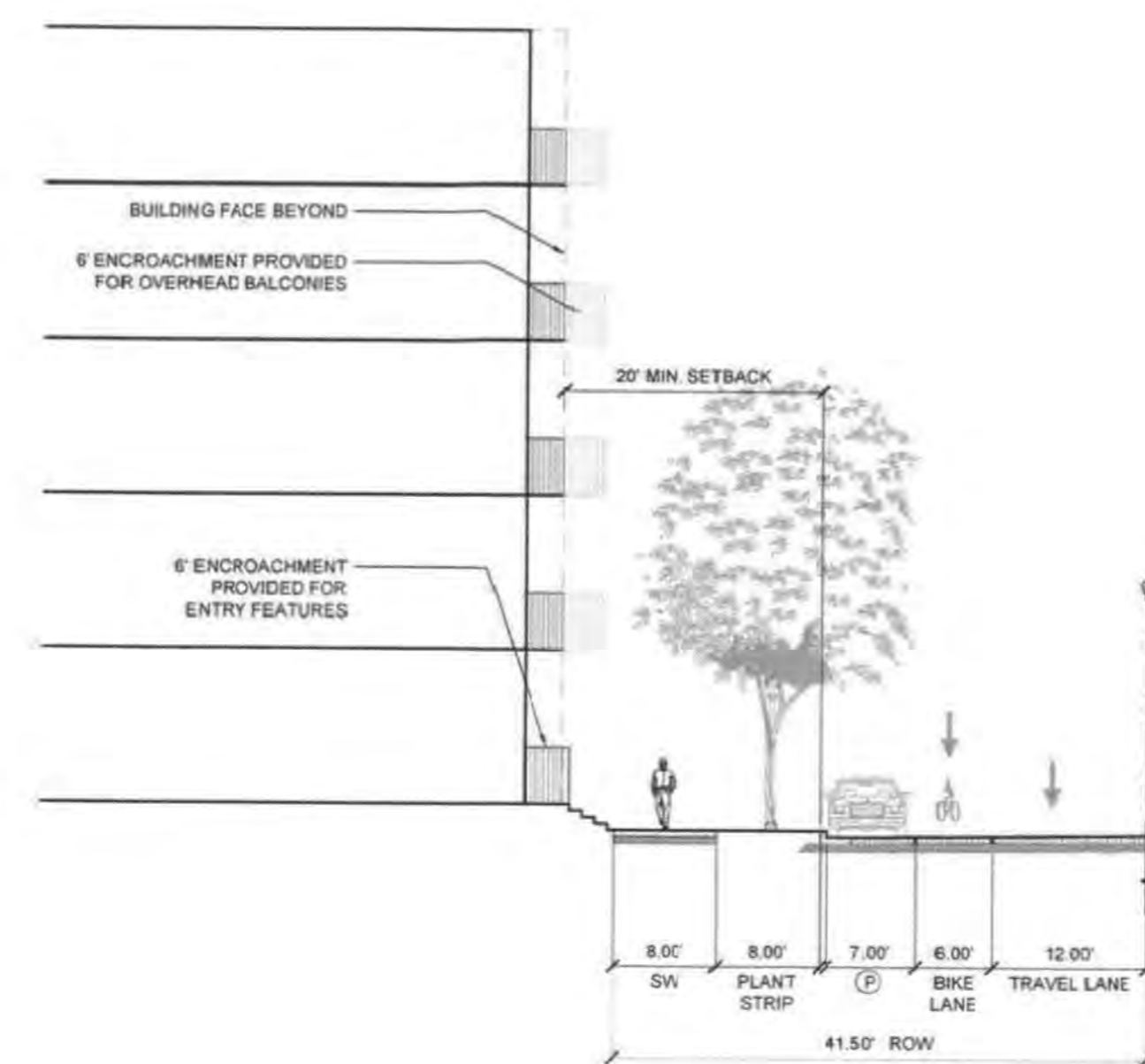
- FIRE PROTECTION**
No optional provisions taken.
- SIGNAGE**
No optional provisions taken.
- LIGHTING**
 - All exterior lights installed on the Site shall be full cut-off type lighting, excluding lower, decorative lighting that may be installed along driveways, sidewalks and parking areas.
 - Pedestrian scale lighting will be installed along the Site's frontage on Philemon Ave as Philemon Ave is improved. The lighting will be spaced as recommended by the lighting engineer.
- PHASING**
No optional provisions taken.
- OTHER**
No optional provisions taken.



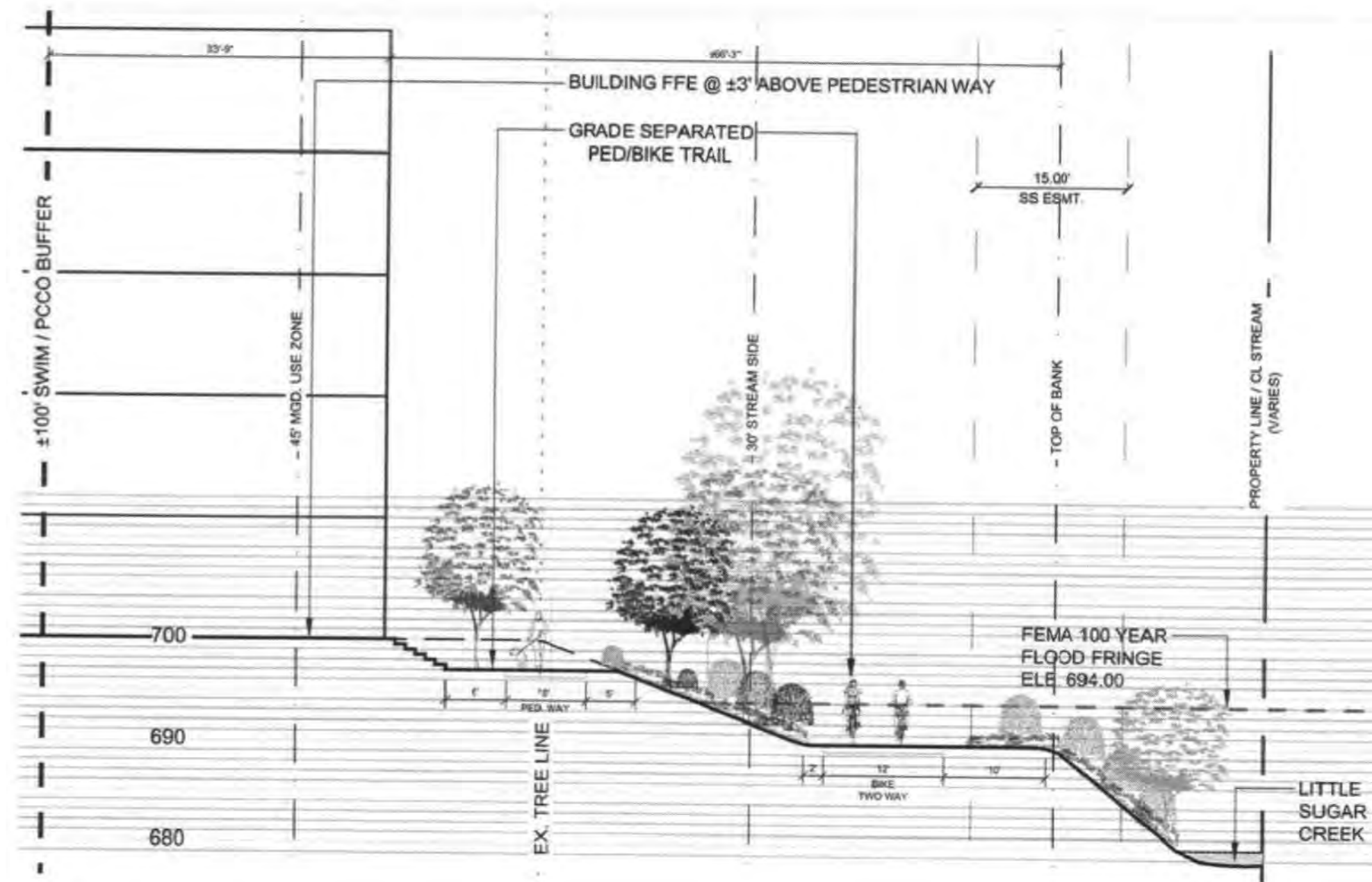
CRAIGHEAD ROAD - CONDITION 1: PARKING



CRAIGHEAD ROAD - CONDITION 2: BUILDING



PHILEMON AVENUE



CROSS CHARLOTTE TRAIL

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 BY: _____

APPROVED BY
 CITY COUNCIL
 JUL 18 2016

NODA GREENWAY SITE
REZONING PETITION No. 2016-79
 FLYWHEEL GROUP

DEVELOPMENT STANDARDS

REVISIONS:
 1. 5/20/16 PER STAFF COMMENTS
 2. 6/23/16 PER STAFF COMMENTS
 3. 7/7/16 ZONING COMMITTEE COMMENTS

DATE: 07/07/16
 DESIGNED BY: _____
 CHECKED BY: _____
 Q.C. BY: _____
 SCALE: _____
 PROJECT #: 4068-001

SHEET #:
RZ-2