



Charlotte-Mecklenburg Planning Department

DATE: February 29, 2020

TO: Sonja Sanders
Zoning Supervisor

FROM: Taiwo Jaiyeoba
Planning Director

SUBJECT: Administrative Approval for Petition No. 2016-134 CapRock, LLC

Attached is the revised site plan for the petition above. This request allows the modification of the proposed site plan as listed below. Since these changes do not alter the intent of the development and meets the requirements of the Charlotte Zoning Ordinance Section 6.207 Alterations to Approval, minor changes, I am administratively approving this plan. Please use these plans when evaluating requests for building permits and certificates of occupancy.

- Modification of design requirements

Staff supports of the request because:

- The site plan complies with the conditional notes and still meets the intent of the original approval by the City Council.

Note:

All other Zoning, PCSO, Subdivision, Tree Ordinances and conditional requirements still apply.

Signage was not reviewed as part of this request.

ADMINISTRATIVE SITE PLAN AMENDMENT PLANS for

ASHLEY FLATS

REZONING PETITION #2016-134

ASHLEY ROAD

CHARLOTTE, MECKLENBURG, NORTH CAROLINA

UTILITY AND GOVERNING AGENCIES CONTACT LIST:

WATER COMPANY

CHARLOTTE WATER
600 E 4TH STREET
CHARLOTTE, NC 28202
(704) 336-7600

SANITARY SEWER COMPANY

CHARLOTTE WATER
600 E 4TH STREET
CHARLOTTE, NC 28202
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FIRE MARSHAL

MECKLENBURG COUNTY/CITY FIRE
JEFF WRIGHT
2145 SUTTLE AVENUE
CHARLOTTE, NORTH CAROLINA 28208
(704) 432-1752

EROSION CONTROL

CITY OF CHARLOTTE ENGINEERING AND PROPERTY MANAGEMENT
JAY WILSON
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POWER COMPANY

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CHARLOTTE, NC 28202
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DEPARTMENT OF TRANSPORTATION

CHARLOTTE DEPARTMENT OF TRANSPORTATION
600 E. 4TH STREET
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CHARLOTTE MECKLENBURG PLANNING COMMISSION
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ZONING DEPARTMENT

MECKLENBURG COUNTY ZONING DEPT.
SHAD SPENCER
700 N. TRYON STREET
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PHONE COMPANY

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(800) 331-0500

GAS COMPANY

PIEDMONT NATURAL GAS
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CHARLOTTE, NC 28217
(800) 752-7504



SHEET INDEX	
Sheet Number	Sheet Title
C0-1	COVER SHEET
RZ-1.0	SITE PLAN
RZ-2.0	BLDG TYPE 1 - ELEVATIONS
RZ-2.1	BLDG TYPE 2 - ELEVATIONS
RZ-2.2	BLDG TYPE 3 - ELEVATIONS
RZ-2.3	BLDG TYPE 3 - ELEVATIONS
RZ-2.4	RENDERING CONCEPT - BREEZEWAY BUILDING

NOTES:

THE SITEWORK FOR THE CLIENT'S PORTION OF THIS PROJECT SHALL MEET OR EXCEED THE REQUIREMENTS OUTLINED HEREIN.

ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO THE ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTIONS OF SERVICES.

ATTENTION IS DRAWN TO THE FACT THAT THE SCALE OF THESE DRAWINGS MAY HAVE BEEN DISTORTED DURING THE REPRODUCTION PROCESS.

THIS DOCUMENT, TOGETHER WITH THE CONCEPTS AND DESIGNS PRESENTED HEREIN, AS AN INSTRUMENT OF SERVICE, IS INTENDED ONLY FOR THE SPECIFIC PURPOSE AND CLIENT FOR WHICH IT WAS PREPARED. REUSE OF AND IMPROPER RELIANCE ON THIS DOCUMENT WITHOUT WRITTEN AUTHORIZATION AND ADAPTATION BY KIMLEY-HORN AND ASSOCIATES, INC. SHALL BE WITHOUT LIABILITY TO KIMLEY-HORN AND ASSOCIATES, INC.

PREPARED BY:

Kimley»»Horn

PROJECT OWNER AND CONSULTANT INFORMATION			
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No.	REVISIONS	DATE	BY

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KHA PROJECT	DATE	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
118129041	11/21/2019	AS SHOWN	JHR	JHR	ARE

COVER SHEET

ASHLEY FLATS
PREPARED FOR
ELMINGTON CAPITAL GROUP
NORTH CAROLINA
CHARLOTTE

SHEET NUMBER
C0-1

Plotted By: Rosenfeld, Jordan Sheet Set: Ashley Flats Site Plan Amendment Layout: C0-1 COVER SHEET January 31, 2020 12:36:04pm K:\CHL\PRJ\118129 - Elmington Capital Group\041 - Ashley\02 - DWG\PlanSheets\Site Plan Amendment\C0-1 COVER SHEET.dwg
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DEVELOPMENT STANDARDS

A. GENERAL PROVISIONS

- THESE DEVELOPMENT STANDARDS FORM A PART OF THE REZONING PLAN ASSOCIATED WITH THE REZONING PETITION FILED BY CARPOCK, LLC ("PETITIONER") TO ACCOMMODATE THE DEVELOPMENT OF A RESIDENTIAL COMMUNITY ON THAT APPROXIMATELY 5.94 ACRE SITE LOCATED ON THE WEST SIDE OF ASHLEY ROAD NORTH OF JOY STREET, WHICH SITE IS MORE PARTICULARLY DEPICTED ON THE REZONING PLAN (THE "SITE"). THE SITE IS COMPRISED OF TAX PARCEL NO. 061-092-02.
- DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE REZONING PLAN, THESE DEVELOPMENT STANDARDS AND THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE (THE "ORDINANCE").
- UNLESS THE REZONING PLAN OR THESE DEVELOPMENT STANDARDS ESTABLISH MORE STRINGENT STANDARDS, THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE UR-2 ZONING DISTRICT SHALL GOVERN THE DEVELOPMENT AND USE OF THE SITE.
- FUTURE AMENDMENTS OR MODIFICATIONS TO THE REZONING PLAN AND/OR THESE DEVELOPMENT STANDARDS MAY BE APPLIED FOR BY THE THEN OWNER OR OWNERS OF THE SITE IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 6 OF THE ORDINANCE. ALTERATIONS OR MODIFICATIONS TO THE REZONING PLAN ARE SUBJECT TO SECTION 6.207 OF THE ORDINANCE.
- THE PETITIONER ACKNOWLEDGES THAT OTHER STANDARD DEVELOPMENT REQUIREMENTS IMPOSED BY OTHER CITY ORDINANCES, STANDARDS, POLICIES, AND APPROPRIATE DESIGN MANUALS WILL EXIST. THOSE CRITERIA (FOR EXAMPLE, ONES THAT REQUIRE BUFFERS, REGULATE STREETS, SIDEWALKS, TREES, STORMWATER, AND SITE DEVELOPMENT, ETC.) WILL APPLY TO THE DEVELOPMENT SITE. CONDITIONS OUTLINED IN THE PETITION ARE SUPPLEMENTAL REQUIREMENTS IMPOSED ON THE DEVELOPMENT IN ADDITION TO THE OTHER STANDARDS. WHERE CONDITIONS ON THIS PLAN DIFFER FROM ORDINANCES, STANDARDS, POLICIES, AND APPROACHES IN EXISTENCE AT THE TIME OF FORMAL ENGINEERING PLAN REVIEW SUBMISSIONS, THE STRICTER CONDITION OR EXISTING REQUIREMENTS SHALL APPLY.
- A MINIMUM OF 50% OF THE TOTAL NUMBER OF MULTI-FAMILY DWELLING UNITS ACTUALLY CONSTRUCTED ON THE SITE SHALL MAINTAIN MONTHLY RENTS THAT ARE INCOME RESTRICTED FOR HOUSEHOLDS EARNING 60% OR LESS, OF THE AREA MEDIAN INCOME FOR A PERIOD OF NOT LESS THAN 15 YEARS FROM THE DATE OF ISSUANCE OF A CERTIFICATE OF OCCUPANCY FOR THE FIRST BUILDING TO BE CONSTRUCTED ON THE SITE.
- THE DEVELOPER IS REQUESTING A CITY OF CHARLOTTE HOUSING LOCATION POLICY WAIVER.

B. PERMITTED USES

- THE SITE MAY BE DEVOTED ONLY TO A RESIDENTIAL COMMUNITY CONTAINING A MAXIMUM OF 113 MULTI-FAMILY UNITS AND TO ANY INCIDENTAL AND ACCESSORY USES RELATING THERETO THAT ARE ALLOWED IN THE UR-2 ZONING DISTRICT. INCIDENTAL AND ACCESSORY USES MAY INCLUDE, WITHOUT LIMITATION, AMENITIES FOR THE RESIDENTS, SUCH AS A CLUBHOUSE AND FITNESS CENTER.
- A CLUBHOUSE BUILDING MAY BE CONSTRUCTED ON THE SITE IN THE GENERAL LOCATION DEPICTED ON THE REZONING PLAN. THE CLUBHOUSE BUILDING MAY CONTAIN, BUT NOT BE LIMITED TO, A LEASING AND MANAGEMENT OFFICE AND AMENITIES SUCH AS A FITNESS CENTER, A RECREATION ROOM AND MEETING SPACE.

C. TRANSPORTATION

- VEHICULAR ACCESS TO THE SITE SHALL BE AS GENERALLY DEPICTED ON THE REZONING PLAN. THE PLACEMENT AND CONFIGURATION OF THE VEHICULAR ACCESS POINTS SHALL BE SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL SITE AND CONSTRUCTION PLANS AND DESIGNS AND TO ANY ADJUSTMENTS REQUIRED FOR APPROVAL BY THE CHARLOTTE DEPARTMENT OF TRANSPORTATION AND/OR THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.
- THE MAXIMUM NUMBER OF PARKING SPACES ALLOWED ON THE SITE SHALL BE LIMITED TO THE MAXIMUM ALLOWED IN THE UR-2 ZONING DISTRICT. THE ON-STREET PARKING SPACES LOCATED ON THE NEW LOCAL PUBLIC STREET TO BE CONSTRUCTED AND INSTALLED ON THE SITE SHALL NOT BE CONSIDERED OR COUNTED WHEN DETERMINING THE MAXIMUM NUMBER OF OFF-PARKING SPACES LOCATED ON THE SITE.
- INTERNAL SIDEWALKS AND PEDESTRIAN CONNECTIONS SHALL BE PROVIDED ON THE SITE AS GENERALLY DEPICTED ON THE REZONING PLAN.
- PETITIONER SHALL CONSTRUCT THE EXTENSION OF HEYWOOD AVENUE THROUGH THE SITE AS GENERALLY DEPICTED ON THE REZONING PLAN. THIS LOCAL PUBLIC STREET SHALL BE BUILT TO THE LOCAL RESIDENTIAL WIDE TYPICAL STREET SECTION AS SPECIFIED IN THE CHARLOTTE LAND DEVELOPMENT STANDARDS.
- A LEFT TURN LANE WILL BE CONSTRUCTED AT THE INTERSECTION OF ASHLEY ROAD AND JOY STREET. FINAL DESIGN SHALL BE DONE DURING PERMITTING.
- BULLARD STREET TO BE BUILT TO "LOCAL RESIDENTIAL WIDE" STANDARD HALF-SECTION ALONG THE SITE FRONTAGE.
- CONSTRUCTION OF ALL TRANSPORTATION IMPROVEMENTS AND THE DEDICATION AND FEE SIMPLE CONVEYANCE OF ALL RIGHTS OF WAY SHALL BE MADE TO THE CITY PRIOR TO ISSUANCE OF THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY.

D. ARCHITECTURAL & SITE DESIGN STANDARDS

- CITY PREFERRED EXTERIOR BUILDING MATERIALS** - ALL PRINCIPAL AND ACCESSORY BUILDINGS ABUTTING A SUBDIVISION REQUIRED PUBLIC OR PRIVATE STREET SHALL COMPRISE A MINIMUM OF 30% OF A BUILDING'S TOTAL FACADE (EXCLUSIVE OF WINDOWS, DOORS, AND BALCONIES).
 - BRICK
 - NATURAL STONE (OR SYNTHETIC EQUIVALENT)
 - OTHER EQUIVALENT OR BETTER MATERIAL APPROVED BY THE PLANNING DIRECTOR OR HIS/HER DESIGNEE.
- THE BUILDING MATERIALS USED ON THE PRINCIPAL BUILDINGS CONSTRUCTED ON THE SITE IS A COMBINATION OF PORTIONS OF THE FOLLOWING BUILDING MATERIALS:** MASONRY, PRECAST CONCRETE, CEMENTITIOUS SIDING, VINYL OR ALUMINUM MATERIAL ON WINDOW SILLINGS AND RAILINGS.
 - THE PROPOSED ROOFING MATERIALS WILL BE ARCHITECTURAL ASPHALT SHINGLES. METAL TYPE ROOFING MATERIALS MAY BE USED ON PORTIONS OF THE ROOFS THAT COVER PORCHES OR BAY WINDOWS.
- PROHIBITED EXTERIOR BUILDING MATERIALS** - THE FOLLOWING EXTERIOR MATERIALS ARE SPECIFICALLY PROHIBITED:
 - VINYL SIDING (EXCEPT FOR SOFFITS AND TRIM INCLUDING WINDOW AND DOOR TRIM)
 - CONCRETE MASONRY UNITS (CMU) NOT ARCHITECTURALLY FINISHED
- BUILDING PLACEMENT AND SITE DESIGN** - BUILDING PLACEMENT AND SITE DESIGN SHALL FOCUS ON AND ENHANCE THE PEDESTRIAN ENVIRONMENT THROUGH THE FOLLOWING STANDARDS:
 - BUILDING STREET FRONTAGE - BUILDINGS SHALL BE ARRANGED AND ORIENTED TO FRONT ALONG ALL NETWORK STREETS (PUBLIC OR PRIVATE STREETS) AS DEFINED BY THE SUBDIVISION ORDINANCE, EXCEPT FOR BUILDINGS 2 AND 3, AND BUILDING 4 AND 5 IF JOY STREET IS NOT OPENED/IMPROVED.
 - BUILDINGS SHALL FRONT A MINIMUM OF 50% OF THE TOTAL STREET FRONTAGE ON THE SITE (EXCLUSIVE OF DRIVEWAYS, PEDESTRIAN ACCESS POINTS, USABLE OPEN SPACE, TREE SAVE AREAS, NATURAL AREAS, AND/OR TREE REPLANTING AREAS).
 - PARKING LOTS SHALL NOT BE LOCATED BETWEEN ANY BUILDING AND ANY PUBLIC OR PRIVATE STREET (EXCEPT FOR THE PARKING AREA LOCATED BETWEEN BUILDINGS 8 AND BULLARD STREET AS GENERALLY DEPICTED ON THE REZONING PLAN).
 - DRIVEWAYS FOR PRIVATE RESIDENTIAL GARAGES AND/OR PARKING SHALL BE PROHIBITED ON ALL NETWORK STREETS.
 - BUILDING ENTRANCES SHALL BE AT OR SLIGHTLY (1' OR MORE) ABOVE GRADE AND SHALL BE HIGHLY VISIBLE AND ARCHITECTURALLY TREATED AS PROMINENT PEDESTRIAN ENTRANCES THROUGH A COMBINATION OF AT LEAST THREE (3) OF THE FOLLOWING FEATURES: (A) DECORATIVE PEDESTRIAN LIGHTING SCENES; (B) ARCHITECTURAL DETAILS CARRIED ABOVE THE GROUND FLOOR; (C) COVERED PORCHES, CANOPIES, AWNINGS OR SUNSHADES; (D) ARCHWAYS; (E) TRANSOM WINDOWS; (F) TERRACED OR RAISED PLANTERS THAT CAN BE UTILIZED AS SEAT WALLS; (G) COMMON OUTDOOR SEATING ENHANCED WITH SPECIALTY DETAILS, PAVING, LANDSCAPING OR WATER FEATURES; (H) DOUBLE DOORS; (I) STOOPS OR STAIRS; AND/OR (J) CONTRASTING PAVEMENT FROM PRIMARY SIDEWALK.
- BUILDINGS SHALL HAVE A MINIMUM 10-FOOT SEPARATION FROM EACH OTHER.**
- BUILDING MASSING & HEIGHT** - BUILDING MASSING SHALL BE DESIGNED TO BREAK UP LONG, MONOLITHIC BUILDING FORMS THROUGH THE FOLLOWING STANDARDS:
 - BUILDING MASSING - BUILDINGS EXCEEDING 120 FEET IN LENGTH SHALL INCLUDE MODULATIONS OF THE BUILDING MASSING/FACADE PLANE (RECESS, PROJECTION, ARCHITECTURAL TREATMENT, ETC.) MODULATIONS SHALL BE A MINIMUM OF 50 FEET WIDE AND SHALL EXTEND OR RECESS A MINIMUM OF 5 FEET, EXTENDING THROUGH ALL FLOORS. MODULATIONS WILL OCCUR EVERY 50 FEET.
 - BUILDING HEIGHT - THE MAXIMUM HEIGHT IN FEET OF EACH BUILDING SHALL BE 40 FEET AS MEASURED FROM THE AVERAGE GRADE AT THE BASE OF EACH BUILDING CONTAINED IN NO MORE THAN 3 STORIES.
- ARCHITECTURAL ELEVATION DESIGN** - ARCHITECTURAL ELEVATIONS SHALL BE DESIGNED TO CREATE VISUAL INTEREST THROUGH THE FOLLOWING STANDARDS:
 - VERTICAL MODULATION AND RHYTHM: BUILDING ELEVATIONS SHALL BE DESIGNED WITH RECOGNIZABLE VERTICAL BAYS OR ARTICULATED ARCHITECTURAL FACADE FEATURES. THE BAYS AND FEATURES MAY INCLUDE, BUT NOT LIMITED TO A COMBINATION OF EXTERIOR WALL OFFSETS, PROJECTION, AND/OR RECESSES, PILASTERS, AND CHANGE IN MATERIALS.
 - BUILDING BASE - BUILDINGS SHALL BE DESIGNED WITH A RECOGNIZABLE BASE. A MINIMUM OF THREE ELEVATIONS OF EACH BUILDING WILL BE ARTICULATED WITH A WAINSCOTT OF PREFERRED EXTERIOR BUILDING MATERIALS LISTED ABOVE OF A MINIMUM OF THREE (3) FEET IN HEIGHT.
 - BLANK WALLS - BUILDING ELEVATIONS FACING PUBLIC OR PRIVATE STREETS SHALL NOT HAVE EXPANSIONS OF BLANK WALLS GREATER THAN 20 FEET.
 - ARCHITECTURAL FEATURES SUCH AS, BUT NOT LIMITED TO, BANDING, MEDALLIONS, BUILDING ARTICULATION, A CHANGE IN MATERIALS WILL BE PROVIDED TO AVOID UNARTICULATED BLANK WALLS.

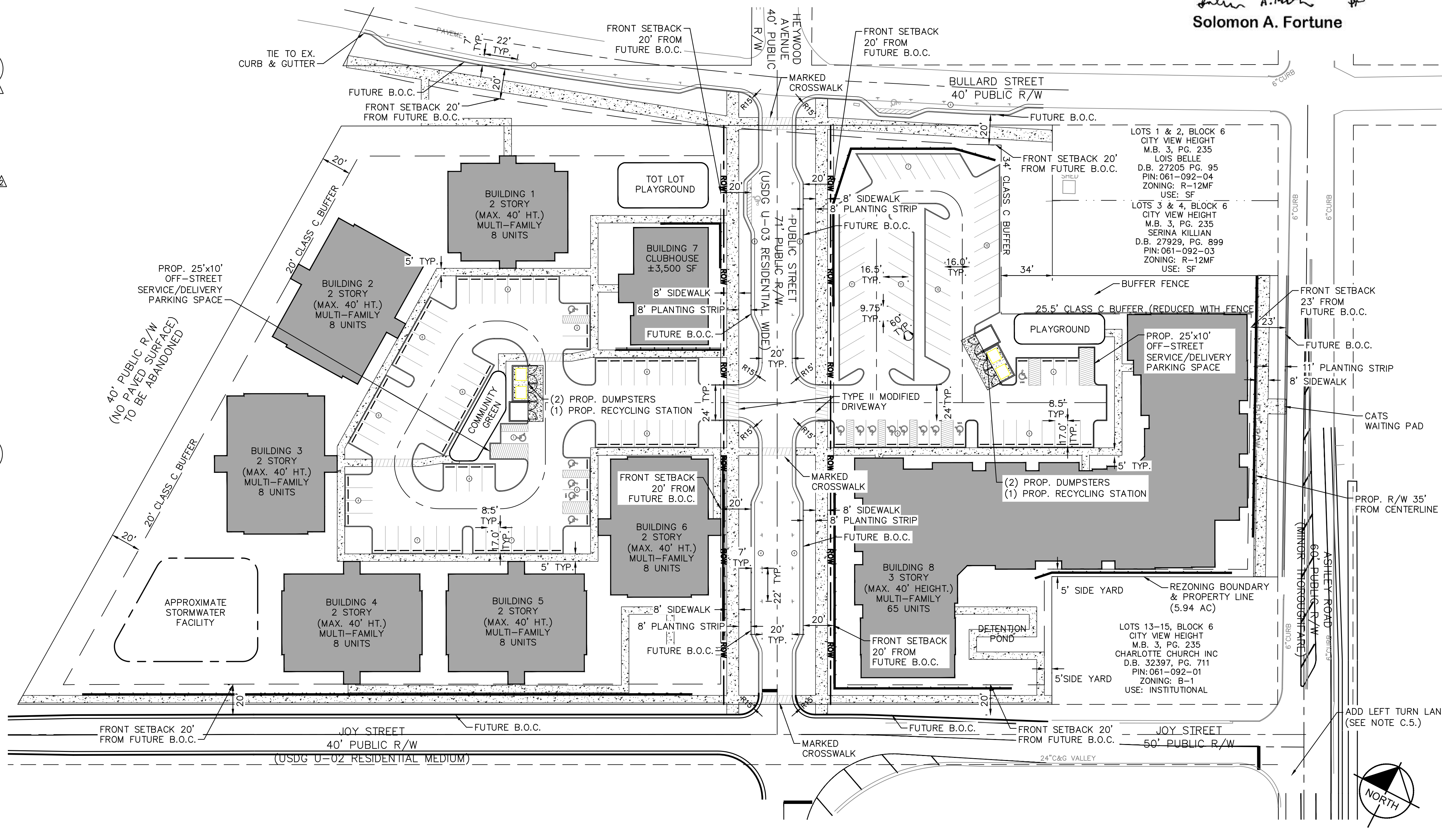
- ROOF FORM AND ARTICULATION - ROOF FORM AND ROOFLINES SHALL BE DESIGNED TO AVOID THE APPEARANCE OF A LARGE MONOLITHIC ROOF STRUCTURE THROUGH THE FOLLOWING STANDARDS:
 - LONG ROOFLINES (PITCHED OR FLAT) SHALL AVOID CONTINUOUS EXPANSIONS WITHOUT VARIATION BY INCLUDING CHANGES IN HEIGHT AND/OR ROOF FORM (E.G. DORMERS, GABLES, ETC.).
 - FOR PITCHED ROOFS THE ALLOWED MINIMUM PITCH SHALL BE 5:12 (FIVE FEET IN VERTICAL HEIGHT FOR EVERY TWELVE IN HORIZONTAL LENGTH), EXCLUDING BUILDINGS WITH A FLAT ROOF AND PARAPET WALLS.
 - ROOF TOP HVAC AND RELATED MECHANICAL EQUIPMENT WILL BE SCREENED FROM PUBLIC VIEW AT GRADE FROM THE NEAREST STREET AND FROM THE NEAREST SINGLE FAMILY STRUCTURE.
 - SERVICE AREA SCREENING - SITE SERVICE AREAS (DUMPSTERS, REFUSE AREAS, RECYCLING, STORAGE) SHALL BE SCREENED FROM THE VIEW THROUGH THE FOLLOWING STANDARDS:
 - SERVICE AREAS WILL BE SCREENED BY A MINIMUM 30 PERCENT MASONRY MATERIAL.
 - UTILITY STRUCTURES NEED TO BE SCREENED ARCHITECTURALLY OR WITH EVERGREEN PLANT MATERIAL.
 - WALLS SHALL BE DESIGNED TO MATCH AND COMPLEMENT THE BUILDING ARCHITECTURE OF THE RESIDENTIAL BUILDINGS OF THE SUBJECT PROPERTY.
 - ALL DUMPSTERS WILL BE SCREENED WITH SOLID ENCLOSURES AND GATES. PETITIONER AGREES TO MEET ALL REQUIREMENTS AS STATED IN CHAPTER 9 AND 12 OF THE CHARLOTTE CITY CODE REGARDING SOLID WASTE, COMPACTOR AND RECYCLING AREAS.
- E. STREETS/SCAPE/LANDSCAPING**
- A 11-FOOT WIDE PLANTING STRIP AND A 8-FOOT WIDE SIDEWALK SHALL BE INSTALLED ALONG THE WEST SIDE OF ASHLEY ROAD AS GENERALLY DEPICTED ON THE REZONING PLAN.
 - CURB AND GUTTER ALONG WITH AN 8-FOOT WIDE PLANTING STRIP AND 8 FOOT WIDE SIDEWALK SHALL BE INSTALLED ALONG THE FRONTAGE OF BULLARD STREET AS GENERALLY DEPICTED ON THE REZONING PLAN. EXACT ALIGNMENT OF THESE IMPROVEMENTS TO BE DETERMINED UPON CONFIRMATION OF CITY RIGHT-OF-WAY CONVEYANCE FROM ADJACENT OWNER(S).
 - AN 8-FOOT WIDE PLANTING STRIP AND AN 8-FOOT WIDE SIDEWALK SHALL BE INSTALLED ALONG THE FRONTAGE OF JOY STREET AS GENERALLY DEPICTED ON THE REZONING PLAN.
 - IN THE CASE THAT THE 40-FOOT UNOPENED RIGHT-OF-WAY IS ABANDONED IN THE FUTURE AS NOTED, THE 20' CLASS 'C' BUFFER AT THE REAR OF THE PROPERTY, ADJACENT TO THAT RIGHT-OF-WAY, MAY BE MOVED FROM THE EXISTING PARCEL INTO THE ABANDONED AREA.
- F. ENVIRONMENTAL**
- DEVELOPMENT OF THE SITE SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY OF CHARLOTTE TREE ORDINANCE. THE DEVELOPER SHALL UTILIZE THE "BUYOUT" OPTION FOR THE DIFFERENCE BETWEEN THE TREE SAVE AREA PROVIDED (0.10 ACRES) AND THE TREE SAVE REQUIRED (0.87 ACRES).
 - THE PETITIONER SHALL COMPLY WITH CITY OF CHARLOTTE ADOPTED POST CONSTRUCTION CONTROLS ORDINANCE.
- G. LIGHTING**
- ALL FREESTANDING LIGHTING FIXTURES INSTALLED ON THE SITE (EXCLUDING LOWER, DECORATIVE LIGHTING THAT MAY BE INSTALLED ALONG THE DRIVEWAYS AND SIDEWALKS AND LANDSCAPING LIGHTING) SHALL BE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED SO THAT DIRECT ILLUMINATION DOES NOT EXTEND PAST ANY PROPERTY LINE OF THE SITE.
 - THE MAXIMUM HEIGHT OF ANY PEDESTRIAN SCALE, FREESTANDING LIGHTING FIXTURE INSTALLED ON THE SITE, INCLUDING ITS BASE, SHALL NOT EXCEED 21 FEET.
 - ANY LIGHTING FIXTURES ATTACHED TO THE BUILDINGS TO BE CONSTRUCTED ON THE SITE SHALL BE DECORATIVE, CAPPED AND DOWNWARDLY DIRECTED.
- H. BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS**
- IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO THE USE AND DEVELOPMENT OF THE SITE IMPOSED UNDER THESE DEVELOPMENT STANDARDS AND THE REZONING PLAN WILL, UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF PETITIONER AND THE CURRENT AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE SUCCESSORS IN INTEREST AND ASSIGNS.
 - THROUGHOUT THESE DEVELOPMENT STANDARDS, THE TERM "PETITIONER" SHALL BE DEEMED TO INCLUDE THE HEIR, DEVISEE, PERSONAL REPRESENTATIVE, SUCCESSORS IN INTEREST AND ASSIGNS OF PETITIONER OR THE OWNER OR OWNERS OF THE SITE FROM TIME TO TIME WHO MAY BE INVOLVED IN ANY FUTURE DEVELOPMENT THEREOF.
 - ANY REFERENCE TO THE ORDINANCE HEREIN SHALL BE DEEMED TO REFER TO THE REQUIREMENTS OF THE ORDINANCE IN EFFECT AS OF THE DATE THIS REZONING PETITION IS APPROVED.

DEVELOPMENT SUMMARY:

TAX PARCEL NUMBER:	061-092-02
SITE ACREAGE:	±5.94 ACRES
EXISTING ZONING:	R-12MF
PROPOSED ZONING:	UR-2(CD) 5-YR VESTED RIGHTS
SETBACKS:	
FRONT:	20' FROM BACK OF CURB (23' AT ASHLEY ROAD)
SIDE:	5'
REAR:	10'
PROPOSED USE:	APARTMENTS
MULTI-FAMILY:	113 UNITS
DENSITY:	19.0 UNITS/ACRE
VEHICULAR PARKING:	
MINIMUM REQUIRED:	
MULTI-FAMILY:	113 SPACES (1 SPACE/UNIT)
MAXIMUM ALLOWED:	
MULTI-FAMILY:	226 SPACES (2 SPACES/UNIT)
PROPOSED:	
MULTI-FAMILY:	144 SPACES
ON-STREET:	29 SPACES
TOTAL:	173 SPACES

Attached to Administrative
Approval

Solomon A. Fortune
Solomon A. Fortune



Kimley»Horn
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KHA PROJECT	118129041
DATE	11/21/2019
SCALE	AS SHOWN
DESIGNED BY	JHR
DRAWN BY	JHR
CHECKED BY	ARE

SITE PLAN

ASHLEY FLATS
PREPARED FOR
ELMINGTON CAPITAL GROUP
NORTH CAROLINA

SHEET NUMBER
RZ-1.0

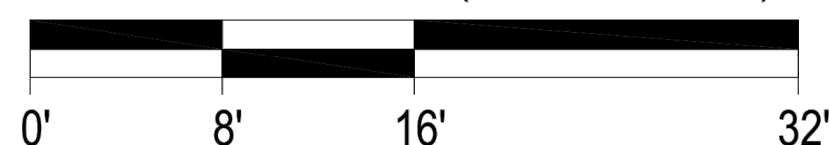


BUILDING TYPE I - FRONT/REAR ELEVATION



BUILDING TYPE I - RIGHT/LEFT ELEVATION

SCALE: 1/8" = 1'-0" (24"x36" SHEET)





BUILDING TYPE II - FRONT/REAR ELEVATION

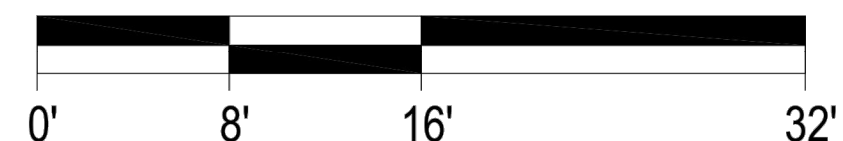


BUILDING TYPE II - RIGHT/LEFT ELEVATION

- COMPOSITION SHINGLES
- SIDING
- STUCCO OR FIBER CEMENT PANEL
- METAL RAILING
- BRICK

- COMPOSITION SHINGLES
- SIDING
- STUCCO OR FIBER CEMENT PANEL
- METAL RAILING
- BRICK

SCALE: 1/8" = 1'-0" (24"x36" SHEET)



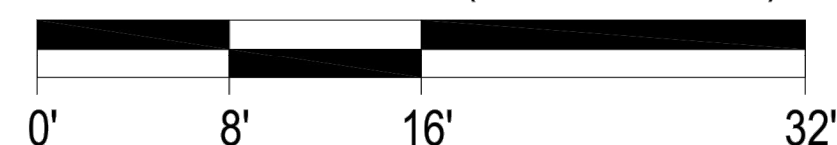


BUILDING TYPE III - FRONT ELEVATION



BUILDING TYPE III - RIGHT ELEVATION

SCALE: 1/8" = 1'-0" (24"x36" SHEET)





BUILDING TYPE III - REAR ELEVATION



BUILDING TYPE III - LEFT ELEVATION

SCALE: 1/8" = 1'-0" (24"x36" SHEET)



