

SURVEY DISCLAIMER

BACKGROUND INFORMATION PROVIDED BY MECKLENBURG COUNTY
GIS AND PLANIMETRICS.

LEGEND
SYMBOL

PROPOSED REZONING BOUNDARY

PROPOSED PROPERTY LINE

PROPOSED SETBACK LINE

APPROXIMATE VEHICULAR CONNECTIONS

PROPOSED LANDSCAPE BUFFER

PROPOSED TREE SAVE AREA

SITE DATA:
TAX PARCELS: 15716201

15716204

*REFER TO DEVELOPMENT STANDARD NOTES ON THIS SHEET FOR

TOTAL SITE ACREAGE: +/- 0.777 AC

PROPOSED PARCEL ZONING: 0-2(CD)
PROPOSED PARCEL ACREAGE: 0.342 AC

EXISTING USES: SINGLE FAMILY

EXISTING ZONING: R-17MF

FURTHER INFORMATION.

TOTAL SITE AREA:

TREE SAVE CALCULATIONS:

TREE SAVE REQUIRED: 0.05 AC
TREE SAVE PROVIDED: 0.076 AC

REVISIONS

NO. DATE DESCRIPTION

1 6/21/18 AMENDED REZONING PLAN

2 12/18/18 REZONING HEARING REVISIONS

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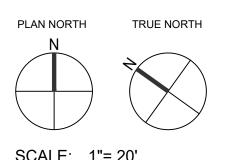
landscape architecture I planning I civil engineering

ISSUED FOR CONSTRUCTION

MANAGING PARTNER (PE): DATE: DATE: DATE: LANDS. ARCHITECT/DESIGNER: DATE: DAT

Craig Ave Rezoning
4101-4113 Craig Ave
Charlotte, NC 28211

REZONING PETITION #: 2018-058



SCALE: 1"= 20'
0 10' 20' 40'

DATE: 04/20/18 MPIC: WLL

DRAWN BY: ESB CHECKED BY: CCB/HKG
PROJECT NUMBER: 00433.00

SCALE: 1"=20'
TITLE:

DEVELOPMENT PLAN &
DEVELOPMENT STANDARD NOTES

RZ-1.0

DEVELOPMENT STANDARDS:

DECEMBER 20, 2018

1. GENERAL PROVISIONS

A. THESE DEVELOPMENT STANDARDS FORM A PART OF THE REZONING PLAN ASSOCIATED WITH THE REZONING PETITION FILED BY WHITE OAK MANAGEMENT, INC. (HEREINAFTER REFERRED TO AS THE "PETITIONER") FOR AN APPROXIMATELY .764 ACRE SITE LOCATED ON THE WEST SIDE OF DELANE AVENUE, EAST OF THE INTERSECTION OF CRAIG AVENUE AND DELANE AVENUE, WHICH SITE IS MORE PARTICULARLY DEPICTED ON THE REZONING PLAN (HEREINAFTER REFERRED TO AS THE "SITE"). THE SITE IS COMPRISED OF PORTIONS OF

TAX PARCEL NOS. 157-162-01, 157-162-02, 157-162-03 AND 157-162-04.

B. DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE REZONING PLAN, THESE DEVELOPMENT STANDARDS AND THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE (THE "ORDINANCE").

C. UNLESS THE REZONING PLAN OR THESE DEVELOPMENT STANDARDS ESTABLISH MORE STRINGENT STANDARDS, THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE 0-2 ZONING DISTRICT SHALL GOVERN ALL DEVELOPMENT TAKING PLACE ON THE SITE.

D. FUTURE AMENDMENTS TO THE REZONING PLAN AND/OR THESE DEVELOPMENT STANDARDS MAY BE APPLIED FOR BY THE THEN OWNER OR OWNERS OF THE SITE IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 6 OF THE ORDINANCE. ALTERATIONS TO THE REZONING PLAN

2. PERMITTED USES/DEVELOPMENT LIMITATIONS

ARE SUBJECT TO SECTION 6.207 OF THE ORDINANCE.

A. THE SITE MAY BE DEVOTED ONLY TO OFF-STREET SURFACE PARKING THAT WILL SERVE THE USES LOCATED ON TAX PARCEL NO. 157-166-15.

3. TRANSPORTATION

A. VEHICULAR ACCESS SHALL BE AS GENERALLY DEPICTED ON THE REZONING PLAN. THE PLACEMENT AND CONFIGURATION OF THE ACCESS POINTS ARE SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED BY THE CHARLOTTE DEPARTMENT OF TRANSPORTATION ("CDOT").

B. THE ALIGNMENTS OF THE INTERNAL VEHICULAR CIRCULATION AREAS AND THE DRIVEWAYS MAY BE MODIFIED BY PETITIONER TO ACCOMMODATE CHANGES IN TRAFFIC PATTERNS, PARKING LAYOUTS AND ANY ADJUSTMENTS REQUIRED FOR APPROVAL BY CDOT IN ACCORDANCE WITH APPLICABLE PUBLISHED STANDARDS.

C. PRIOR TO USE OF THE PARKING LOT TO BE CONSTRUCTED ON THE SITE,
PETITIONER SHALL DEDICATE AND CONVEY TO THE CITY OF CHARLOTTE
THOSE PORTIONS OF THE SITE LOCATED IMMEDIATELY ADJACENT TO
DELANE AVENUE AS REQUIRED TO PROVIDE RIGHT OF WAY MEASURING,
WHERE FEASIBLE, TO TWO FEET BEHIND BACK OF SIDEWALK AS MORE
PARTICULARLY DEPICTED ON THE REZONING PLAN, TO THE EXTENT THAT
SUCH RIGHT OF WAY DOES NOT ALREADY EXIST.

D. ALL REQUIRED TRANSPORTATION IMPROVEMENTS SHALL BE CONSTRUCTED AND APPROVED PRIOR TO USE OF THE PARKING LOT TO BE CONSTRUCTED ON THE SITE.

4. STREETSCAPE/LANDSCAPING/BUFFERS

A. BUFFERS SHALL BE ESTABLISHED ON THE SITE AS REQUIRED BY THE ORDINANCE AND AS DEPICTED ON THE REZONING PLAN, AND SUCH BUFFERS SHALL CONFORM TO THE STANDARDS OF SECTION 12.302 OF THE ORDINANCE. THE WIDTH OF ANY REQUIRED BUFFER MAY BE REDUCED BY 25% IF A WALL, FENCE OR BERM IS PROVIDED THAT MEETS THE REQUIREMENTS OF SECTION 12.302(8) OF THE ORDINANCE.

B. IN THE EVENT THAT AN ADJACENT PARCEL OF LAND IS EITHER REZONED TO A ZONING DISTRICT OR DEVOTED TO A USE THAT ELIMINATES OR REDUCES THE BUFFER REQUIREMENTS ON THE SITE, PETITIONER MAY REDUCE OR ELIMINATE, AS THE CASE MAY BE, THE RELEVANT BUFFER AREAS ACCORDINGLY

C. SIDEWALKS AND PLANTING STRIPS SHALL BE INSTALLED ALONG THE SITE'S PUBLIC STREET FRONTAGE AS GENERALLY DEPICTED ON THE

5. ENVIRONMENTAL

A DEVELOPMENT OF THE SITE SHALL COMPLY WITH THE CITY OF

A. DEVELOPMENT OF THE SITE SHALL COMPLY WITH THE CITY OF CHARLOTTE TREE ORDINANCE.

6. LIGHTING

REZONING PLAN.

A. ANY FREESTANDING LIGHTING FIXTURES INSTALLED ON SITE SHALL HAVE A MAXIMUM HEIGHT OF 21 FEET, AND ALL SUCH FREESTANDING LIGHTING FIXTURES SHALL BE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED SO THAT DIRECT ILLUMINATION DOES NOT EXTEND PAST ANY PROPERTY LINE OF THE SITE.

7. BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS

A. IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO THE USE AND DEVELOPMENT OF THE SITE IMPOSED UNDER THESE DEVELOPMENT STANDARDS AND THE REZONING PLAN WILL, UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF PETITIONER AND THE CURRENT AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE SUCCESSORS IN INTEREST AND ASSIGNS.

B. THROUGHOUT THESE DEVELOPMENT STANDARDS, THE TERM "PETITIONER" SHALL BE DEEMED TO INCLUDE THE HEIRS, DEVISEES, PERSONAL REPRESENTATIVES, SUCCESSORS IN

INTEREST AND ASSIGNS OF PETITIONER OR THE OWNER OR OWNERS OF THE SITE FROM TIME TO TIME WHO MAY BE INVOLVED IN ANY FUTURE DEVELOPMENT THEREOF.

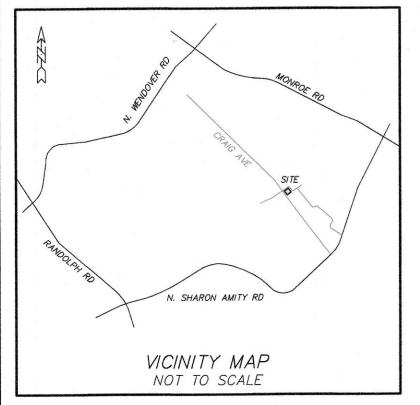
ANY REFERENCE TO THE ORDINANCE HEREIN SHALL BE DEEMED TO REFER TO THE REQUIREMENTS OF THE ORDINANCE IN EFFECT AS OF THE DATE THIS REZONING PETITION IS APPROVED.

1. CONTRACTOR IS RESPONSIBLE FOR PLACEMENT OF ALL BARRICADES, SIGNAGE, FLAGGERS, SHORING, ETC., TO ENSURE THE SAFETY OF WORKERS

2. ALL PAVEMENT CUTS SHALL BE REPLACED ACCORDING TO NORTH CAROLINA DEPARTMENT OF TRANSPORTATION AND CHARLOTTE WATER.

AND THE PUBLIC.

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LEGEND:

EIP = EXISTING IRON PIN OIP = OLD IRON PIPE SIP = SET IRON PIN R/W = RIGHT OF WAYAC = AIR CONDITIONING PWR = POWER PAD PM = POWER METER PP = POWER POLE

LP = LIGHT POLE GM = GAS METERTELE = TELEPHONE PEDESTAL

FES = FLARED END SECTION RCP = REINFORCED CONCRETE PIPE CMP = CORRUGATED METAL PIPE CPP = CORRUGATED PLASTIC PIPE PVC = POLYVINYL CHLORIDE DU = DESTINATION UNKNOWN SDE = STORM DRAINAGE EASEMENT SDMH = STORM DRAIN MANHOLE

CB = CATCH BASIN YI = YARD INLET DI = DROP INLET

CATV = CABLE TELEVISION WM = WATER METER

SSE = SANITARY SEWER EASEMENT SSMH = SANITARY SEWER MANHOLE CO = SEWER CLEAN OUT BC = BACK OF CURB

SSMH (S) SANITARY SEWER MANHOLE SDMH D STORM DRAIN MANHOLE LOCATED DECIDUOUS TREE LOCATED EVERGREEN TREE LP C LIGHT POLE PP & POWER POLE BFP - BACK FLOW PREVENTOR GW- GUY WIRE CB CATCH BASIN DI DROP INLET

_____x ____x ____x ____ FENCE

____ STORM DRAIN PIPE OVERHEAD ELECTRIC ---s---s---s---s---s---sANITARY SEWER PIPE GAS GAS UNDERGROUND GAS

SIP SET IRON PIN

GV GAS VALVE

WV WATER VALVE

MB & MAIL BOX

W WELL

WM WATER METER

FH FIRE HYDRANT

EIP EXISTING IRON PIN

CP ⊗ CALCULATED POINT

BOUNDARY LINE

- - - - ADJOINING LINE (NOT SURVEYED)

€: 715.79' IN: 711.79' OUT: 709.99'

BACK OF CURB SSMH RIM: 725.45' DeLANE AVENUE 40' RIGHT OF WAY PER MB 3/193 RIM: 719.80' IN: 713.60' QUT: 713.30' - IN: 716.27' ----s --- 8" PVC --S.36 49 30 E 49.98 RIGHT OF WAY S36 23 38 E 50.05 S36°48'34"E 49.66 EDGE OF GRAVEL PARKING LOT 20' SETBACK 20' SETBACK 20' SETBACK 20' SETBACK LOTS 15 LOTS 17 LOTS 13 LOTS 19 & 18 & 16 & 14 & 20 BLOCK BLOCK F BLOCK F 0.195 ACRE 0.195 ACRE 0.195 ACRE 0.194 ACRE #4109 #4113 ~NOW OR FORMERLY~ ~NOW OR FORMERLY~ WHITE OAK MANOR INC ~NOW OR FORMERLY~ WHITE OAK MANOR INC ~NOW OR FORMERLY~ WHITE OAK MANOR INC WHITE OAK MANOR INC DB 30933/305 PID 15716204 DB 26647/843 DB 20010/318 PID 15716201 PID 15716202 FENCE LOT 22 MB 49/791 ~NOW OR FORMERLY~ SHANNON LYNCH DB 20708/786 PID 15716220 #4109 1 STORY BRICK 1 STORY BRICK 1 STORY BRICK 1 STORY BRICK COMP ROOF COMP ROOF ψ'ψ£ 722.66' RIM: 721.84' CONC. WALK CONC. WALK CONC. WALK 20' SETBACK 20' SETBACK 20' SETBACK RIM: 717.61' N36°54'06"W ! N36°48'24"W 50.00 50.Q0 49.99 50.00 SSMH
RIM: 713.49'
BACK OF CURB
12" RCP (FOUND) CRAIG AVENUE RIM: 718.63' 50' RIGHT OF WAY PER MB 3/193 EDGE OF PAVEMENT

GRAPHIC SCALE

(IN FEET)

1 inch = 20 ft.



21 NCAC 56 .1604: MAP CERTIFICATION

I CERTIFY THAT THIS MAP WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION (TITLE REFERENCES AS SHOWN ON PLAT); THAT THE BOUNDARIES NOT SURVEYED ARE INDICATED AS DRAWN FROM TITLE REFERENCES AS SHOWN ON PLAT; THAT THE RATIO OF PRECISION OR POSITIONAL ACCURACY MEETS OR EXCEEDS 1:10,000; AND THAT THIS MAP MEETS THE REQUIREMENTS OF THE STANDARDS OF PRACTICE FOR LAND SURVEYING IN NORTH CAROLINA (21 NCAC 56. 1600).

PROFESSIONAL LAND SURVEYOR



Z N W 0 2 Z,

WAL 0 20, ME

UNDERGROUND UTILITIES MARKED BY: RDL PRIVATE UTILITY LOCATING 6012 BAYFIELD PARKWAY CONCORD, NC 28027 (704) 492-4841 RDLLOCÁTING@GMAIL.COM

NOTES:

1. IRON PINS SET AT ALL CORNERS UNLESS OTHERWISE NOTED.

2. PROPERTY ZONED: R-17(MF) MINIMUM SETBACK 20' MINIMUM SIDE YARD 5' MINIMUM REAR YARD 20'

(SETBACKS & ZONING MATTERS ARE SUBJECT TO INTERPRETATION BY LOCAL MUNICIPALITIES) 3. TAX PARCEL NUMBERS 15716201, 15716202, 15716203, & 15716204.

4. DEED REFERENCE: DB 20010 PG 318, DB 26647 PG 843, DB 26675 PG 14, & DB 30933 PG 305.

5. BOUNDARY SURVEY ONLY THROUGH POINTS AS SHOWN.

6. THIS PROPERTY IS LOCATED WITHIN AN AREA HAVING A ZONE DESIGNATION X BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA), ON FLOOD INSURANCE RATE MAP (FIRM) NO. 3710456200K, WITH A DATE OF IDENTIFICATION OF 02/16/2014.

7. THIS PROPERTY MAY BE SUBJECT TO ADDITIONAL RECORDED OR UNRECORDED EASEMENTS, RIGHTS-OF-WAY, OR RESTRICTIVE COVENANTS, OTHER THAN SHOWN. SURVEY MADE WITHOUT THE BENEFIT OF A TITLE

8. AREA COMPUTED BY COORDINATED METHOD.

9. NO NCGS MONUMENT FOUND WITHIN 2000'. 10. UTILITY LOCATIONS SHOWN ARE LOCATED BASED ON SITE CONDITIONS AND MARKINGS AT THE TIME OF SURVEY. CONTRACTORS ARE TO HAVE ALL UTILITIES ACCURATELY MARKED PRIOR TO CONSTRUCTION.